

PRCUTS STAGE 2 KINGS BAY PRECINCT

Master Plan Report

We acknowledge the traditional custodians of the land, and pay our respects to their elders past present and emerging, recognising their continuing connection to land, waters and culture.

Issue	Title	Date	Prepared	Checked
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1.0 INTRODUCTION

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1.1 INTRODUCTION

Project Background

The Parramatta Road Corridor Urban Transformation Strategy sets out a 30 year plan for development located along the Parramatta Road corridor, extending 20km from Camperdown to Granville.

The Parramatta Road Corridor Homebush, Burwood, and Kings Bay precincts will feature a highly accessible and permeable public domain, with enhanced transport options, a greater degree of local amenity, and a thorough integration with existing proposals throughout the study area.

The Parramatta Road Corridor Urban Transformation stage 1 included the proposed development of Homebush North, Burwood, and Kings Bay, with stage 2 extending the vision which integrates this existing proposal, and seeks opportunities for growth throughout the rest of these precincts.

Our approach to the PRCUTS stage 2 Urban Design Review and Master Plan is to understand each precinct on an individual level to uncover strengths, opportunities, and distinct characteristics, whilst offering potential improvements to achieve the best urban design outcomes through:

- Improved access to public transport options.
- A pedestrian friendly public domain.
- Introduce a more diverse housing and land use mix.
- Understanding of local culture and heritage.
- Facilitating quality open and recreational space.

The Kings Bay Precinct is located between the activity centres of Burwood and Five Dock, consisting of primarily industrial and commercial premises. It comprises a diversity of existing land uses, including low-density residential dwellings, a productive heart along Parramatta Road, and various community centres and schools.

Stage 2 extends the existing Stage 1 work, east and west along Parramatta Road.

Under the PRCUTS, the vision for the Kings Bay Precinct is a lively residential urban village, with a dense network of local streets and open space, and an identity built upon its proximity to the Sydney Harbour.

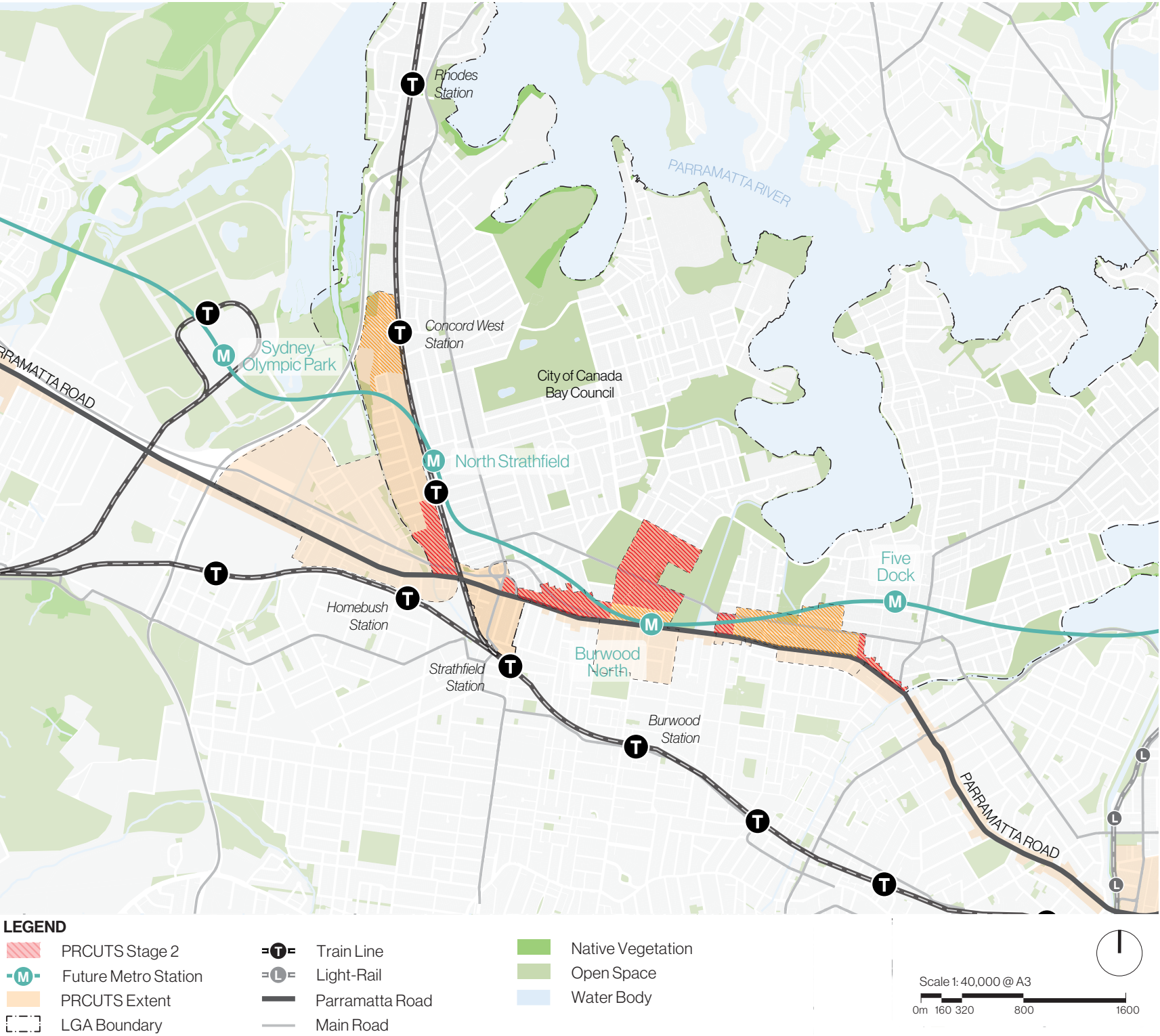
Study Process

The project has been undertaken over two phases, which constitute the development of the PRCUTS Stage 2 Masterplan:

- Phase 1 - Baseline Analysis
- Phase 2 - Urban Design Review and Masterplan

Phase 1 Baseline Analysis provides a background briefing of the project, summarises the strategic context and site analysis to establish opportunities and constraints to be further developed to as part of phase two urban design review and master plans.

Phase 2 Urban Design Review and Masterplan includes a thorough study of the existing PRCUTS proposed controls for the Stage 2 precinct study areas. A process of built form testing, which takes into consideration existing controls, regulations, and best-practice urban design will inform the Stage 2 Masterplan.



1.2 COUNTRY

We acknowledge the traditional custodians of the land upon which the site is located, the Wangal people of the Eora nation, their connection to Country, land, water, community and spirit.

We pay respect to Elders past, present and emerging.

The Wangal people knew their traditional lands as ‘Wanne’, which extended north from Burramattagal throughout what is today Sydney’s inner-west.

The people of the Eora nation have an enduring relationship with their traditional lands which extends back tens of thousands of years, and continues into the present day.

Neighbouring Darug and Eora people were the Gadigal to the east, the Wallumattagal on the northern shore of the Parramatta River and the Bediagal to the south..

This relationship with Wanne was based on the principles of custodianship, in which the Wangal people lived in and cared for their traditional lands, as it provided for them in return.

Examples of this enduring relationship are found throughout the Canada Bay region, with rock shelters and middens, bora rings and artefacts relating to the daily lives of the Wangal people displaying a close relationship with the land.

Being a coastal people with access to the Parramatta River or ‘Burramattagal’, fish and crustaceans composed much of their annual diet, while the region’s dense bushland forests and watering holes provided essential resources and medicines.

Nearby estuarine ecosystems provided Aboriginal communities with resources for food and tool manufacture as well as a means of travelling throughout the region by water.

Burramattagal provided the means for a wide trade network which extended throughout the Cumberland Plains, as the Wangal gathered and exchanged goods with surrounding peoples.

Parramatta Road is believed to have been built on one such route which brought together the indigenous people of the Sydney basin, with strong ties to their history and story telling, known as a song-line.

The Wangal people are remembered today immortalised in the names and gathering places found throughout Wanne, though European colonisation in the 18th century sought to break this relationship, their impact endures and shapes the past, present, and future of Canada Bay.



Murama Healing Space, Newington Armoury, Wangal Country. Source: Tranby National Indigenous Adult Education

1.3 POST-SETTLEMENT

Parramatta Road opened in 1811 to link the Sydney colony to its western settlements and the growing township of Parramatta.

It is one of Sydney’s oldest roads and the first road in Australia to be built between two distinct settlements, around which much of the city’s later development would occur.

Though the early route was poorly built, it would be formalised in the early 19th century and opened to public use, funded in part by a toll and by those who were beginning to travel into the colony’s unexplored inner reaches.

Parramatta Road would quickly become one of the colony’s busiest thoroughfares, transporting regional goods from Sydney’s bread-bowl in Parramatta to its metropolitan centre. A stagecoach was introduced in 1823 to transport pedestrians, a journey which would take an hour and a half and was fraught with many potential dangers.

Settlements would begin to flourish on the banks of Parramatta Road as accommodation, shops, services, and churches would seek to provide for those travelling between the two settlements, and those who had established small-holdings in the lands they had travelled.

These settlements would form many of the suburbs and urban centres found along the road today, including Burwood, Strathfield, and Ashfield.

With the introduction of the Parramatta-Sydney rail line in 1855, residential development throughout the region increased sporadically, as now residents were able to traverse Sydney’s rural west with relative ease.

As motorisation began in the early 20th century, Parramatta Road transformed from a trade route into a highly congested thoroughfare, which required significant road widening, and led to a huge population increase alongside the process of Sydney’s suburbanisation.

As a result, commercial activity along Parramatta Road would wane, and while its outlying urban centres would continue to thrive, shops along the road’s extent would experience much less activity, due to the significant congestion, pollution, and use mainly a through-way.

Proposals originating in the late 20th century envision an enlivened corridor which brings the same life to the region experienced during its early years.

Five Dock and the Kings Bay Precinct

Five Dock is first referenced in 1805 and refers to the Five Dock Bay, though the suburb would take its name from a land-holding owned by John Harris, known as Five Dock Farm.

This 600 hectare property comprised today’s Drummoynes, Russell Lea, and Abbotsford Areas, and was used primarily for agricultural output to support the growing Sydney colony.

It was sold in 1838 to Samuel Lyons who built many of the first thoroughfares in the region, then selling off lots to incoming settlers who would proceed to establish small-holdings and market gardens.

As these lands were further subdivided and the local population increased, community facilities including the Five Dock Public School and Rosebank College were founded, these being some of the area’s oldest and most established educational centres.

Major urbanisation would not occur until the early 20th century, as the Parramatta-Sydney rail line, the ‘Emu’ ferry service, and private bus companies would link Five Dock to the metropolitan city centre, and the suburb’s population would rapidly increase.

Local government was established in 1871 as the Municipality of Five Dock, which in 1902 would conjoin to become the City of Drummoynes, and later in 2000 become the City of Canada Bay, representing Five Dock among many of the region’s other urban settlements.

Throughout Parramatta Road, Five Dock has maintained a relatively more industrial economy, though in the 21st century this has shifted towards a more modern economy, with commercial centres replacing previously manufacturing oriented plants.



Parramatta Road, circa. 1879-1899, Source: State Library of NSW



Rosebank College, Source: Rosebank College

1.4 ECOLOGY

The City of Canada Bay is located along the banks of the Parramatta River, and is home to a diversity of riverine, bushland, and plains ecological communities.

The most common extant ecological community found in Canada Bay is the Sydney Turpentine-Ironbark Forest, characterised by its tall canopy located on Wianamatta shales.

Remnants of this are found throughout Canada Bay, most prominently so in Queen Elizabeth Park, on schools grounds, and in the many reserves within its residential neighbourhoods.

Along the Parramatta River and its many minor tributaries which run deep into the landscape, more coastal and riverine ecological communities are common, with the revegetation efforts of Iron Cove Creek encouraging Floodplain Forests and Coastal Saltmarshes to flourish.

The wetlands of Powells Creek were once common throughout the entire extent of the Parramatta River, supporting a highly diverse community of terrestrial and oceanic fauna.

Examples of these wetlands still remain intact throughout Homebush's north and are integrated into the walking tracks and cycling paths in and around Newington Armoury.

City of Canada Bay's environmental strategies prioritise the preservation of these extant ecological communities, and the integration of them into the city's existing and future urban canopy and open spaces.

Preservation of remnant fauna through community guided sustainability goals aim to minimise the impact of the city's growing urban community on the many native mammals, reptiles, and fish that still call this region home.



Revegetation process of Powells Creek within the Homebush Precinct



Turpentine Ironbark Forest found in Queen Elizabeth Park, adjacent to the Burwood Precinct -



Recreational open space with plains flora found in Powells Creek in the north of the Homebush Precinct





2.0 POLICY CONTEXT

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2.1 PARRAMATTA ROAD CORRIDOR URBAN TRANSFORMATION STRATEGY

The Parramatta Road Corridor Urban Transformation Strategy is the long-term vision for developing population and employment growth in the Parramatta Road Corridor.



Parramatta Road Corridor Urban Transformation Strategy

Combining big picture considerations, a sub-regional response, and the depth of local knowledge required to plan for existing and future communities, it will lead to the enlivening of this important Sydney artery.

The Strategy is supported by the Implementation Tool Kit – four documents that will guide and inform how the Strategy is to be implemented, and are summarised on the following pages.

While the Strategy does not directly rezone land, it establishes the framework for land use and transport planning to guide, coordinate and facilitate changes to local planning controls that will lead to the Corridor's transformation. The Strategy will be implemented through planning proposals prepared by landowners or developers, comprehensive local environmental plan reviews undertaken by councils, and State environmental planning policies for future Priority Precincts.

The strategy sets out a vision for the corridor of tomorrow:

- housing choice and affordability
- diverse and resilient economy
- accessible and connected
- vibrant communities and places
- green spaces and links
- sustainability and resilience
- delivery

Relevance to project:

The Strategy sets an overarching vision and high level employment and dwelling projections that are summarised in the adjacent pages. It also identifies a number of challenges to achieving these, which have been synthesised graphically onto maps in the next chapter.



Kings Bay Precinct

Kings Bay Precinct will be a new residential and mixed use urban village on Parramatta Road, with an active main street and strong links to the open space network along Sydney Harbour.

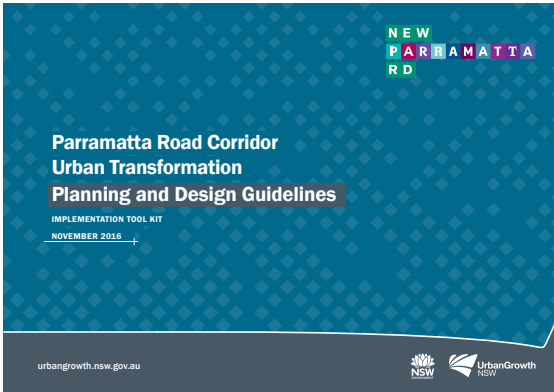
Spencer Street will form the basis of a new and compact local centre – an east-west axis for local shops and services, and a new address for medium and high-density residential development. Taller residential buildings will mark the centre of the Precinct at the corner of Parramatta Road, William Street and Spencer Street. The scale of development will gradually decrease towards adjacent residential areas and Rosebank College.

Existing residential areas will be reimagined to suit the higher level of population density envisioned at the periphery of the Kings Bay Precinct, including low-mid rise multi-dwellings on Taylor Street and Walker Street, as well as Courland Street within the study area's east adjacent to existing businesses.

- Population: 5,200 by 2050
- Homes: 2,500 by 2050
- Jobs: 2,900 by 2050

2.2 PRCUTS IMPLEMENTATION TOOL KIT

The Strategy is supported by the Implementation Tool Kit – four documents that will guide and inform how the Strategy is to be implemented, and are summarised on the following pages.



PRCUTS Planning and Design Guidelines

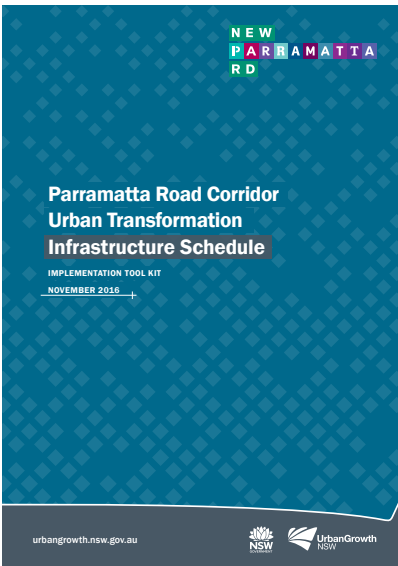
The purpose of the Parramatta Road Corridor Planning and Design Guidelines is to:

- describe the priorities and principles that will ensure future development achieves high design quality and design excellence
- guide the rapidly changing character of the Corridor whilst ensuring future development responds to the distinct character and identity along different parts of the Corridor

The Guidelines have been prepared as planning and development controls significantly differ across the local government areas that make up the Corridor, and have been developed to assist designers and planners apply 'better practice' design principles to promote high quality public, private amenity and good design.

Relevance to project:

- The controls identified in the PRCUTS guidelines will be tested throughout the master plan process and recommendations made at the conclusion of the study.
- The guidelines also identify a number of public domain requirements, strategic links, opportunities and constraints for each precinct, which have been identified and graphically represented for each precinct in later chapters.



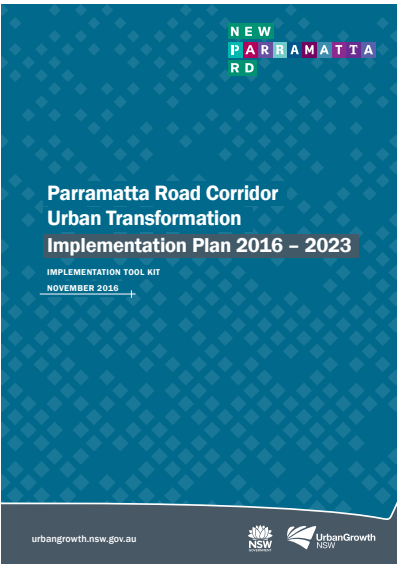
Infrastructure Schedule

Identifies infrastructure in the Corridor must respond to population growth and change.

It also recognises that some existing infrastructure is ageing or is insufficient to meet the needs of communities as they grow and change. The Infrastructure Schedule therefore identifies the transport, open space, community, education and health facilities required to support the proposed growth across the Corridor. It will also assist the coordination of infrastructure and services provided by state agencies, government-owned corporations, local government and the private sector.

Relevance to project:

- Community infrastructure required for the Kings Bay Precinct outlined by Council will be integrated into the Master Plan as public domain improvements or suggested opportunities to be explored through future development in partnership with land-owners.

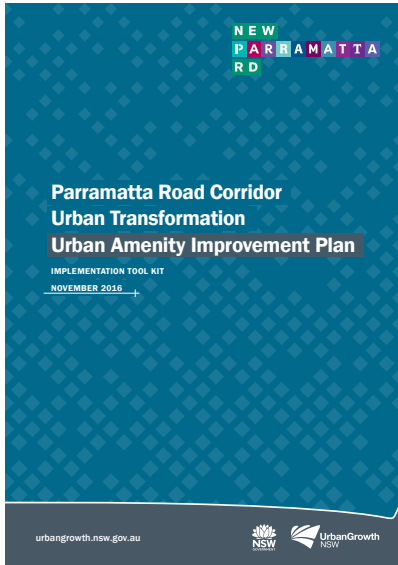


Implementation Plan

Intended to inform and guide the land use planning and development decisions in the Corridor in the short term.

Relevance to project:

- Development in the Precinct to be designed to deliver prioritised pedestrian links and through-site links as indicated in the Planning and Design Guidelines including:
 - + Response to walking link which culminates on the western side of Walker Street adjacent to Lucas Gardens School
 - + Proposed cycle link through Walker Street and along Parramatta Road towards Lucas Road
- New open space at the study area's southern site boundary located at intersection of Henley Marine Drive and Great N Road.



Urban Amenity Improvement Plan

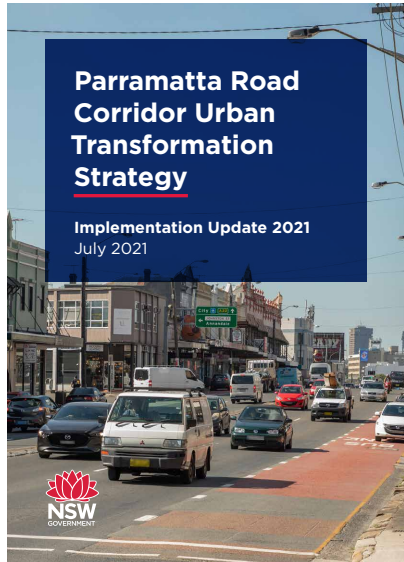
Identifies ways to deliver tangible public benefits, contributing to the delivery of liveable communities and neighbourhoods and stimulate the development.

The works fall into three categories:

- Streetscape upgrade
- Creation of new or improved open spaces, urban plazas and town squares
- New walking and cycling links to key transport nodes

Relevance to project:

- Improvements to Concord Oval as a key recreational asset, located within close proximity of Taylor Street at the study area's eastern site boundary.

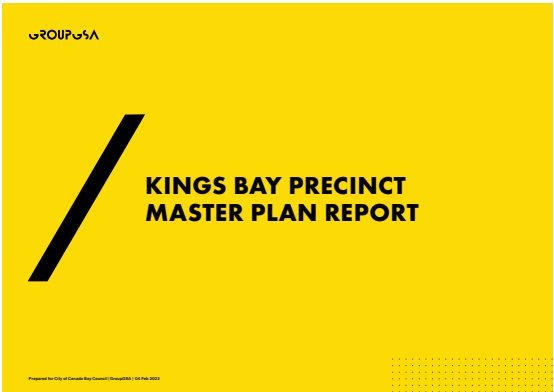


Implementation Update 2021

Supplemented Implementation Plan 2016 - 2023 with additional actions.

Relevance to project:

- Kings Bay Precinct proximity to Sydney Metro West Five Dock and Burwood North stations announced in 2019.
- Transport for NSW improved public transport infrastructure within the corridor and along Parramatta Road may adjust road widths and acquisitions at the Parramatta Road interface.



Stage One Kings Bay Precinct Master Plan Report

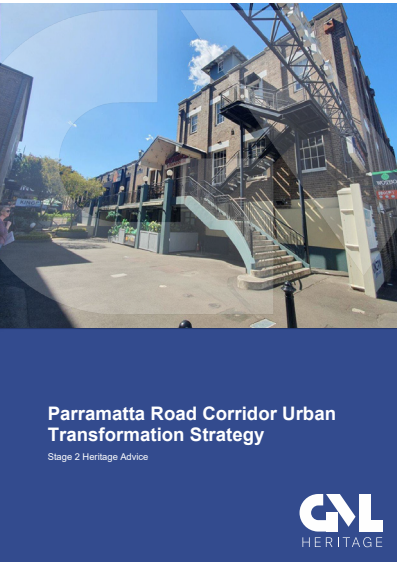
Guides development within the Stage One study area of the Kings Bay Precinct, determining built form controls and amalgamating Council strategies with PRCUTS outcomes.

The scope of the Stage One Master Plan extends from Walker Street to Harris Road, and includes the Kings Bay Precinct town centre and peripheral medium-high density residential clusters. This report determines the precinct's future land use mix, built form controls, active frontages, and location of open space. It functions to inform the Kings Bay Special Precinct section of the Canada Bay DCP, and will amend the Canada Bay LEP 2013.

The Stage One Master Plan was exhibited in February 2022 and is subject to further change pending stakeholder and community input, land-owner submissions, and future studies.

Relevance to project:

- The future built form controls outlined in the Stage One Kings Bay Precinct Master Plan will inform the development of surrounding areas and that included in the Stage Two study area.
- Integration with the Master Plan to create a cohesive overall Kings Bay Precinct will require an understanding of the precinct's future character.
- Built form controls proposed in Stage Two will reflect Council's desired outcomes for the precinct, and respond to shared interfaces with the Stage One study area.
- The Stage Two masterplan is largely an east-west extension of the Stage One masterplan.



Parramatta Road Corridor Urban Transformation Strategy - Stage 2 Heritage Report

This report provides heritage advice on the management of identified heritage items and conservation areas within and in the vicinity of the Stage 2 areas. This report:

- Outlines the process and inputs to date and provides justification for the continued protection of identified heritage significance in the context of future increased density.
- Identifies opportunities for planning and urban development approaches that are sympathetic to existing heritage contexts.
- Describes high-level heritage conservation policy that can be applied within each study area to protect and enhance heritage significant items, places and areas.

Relevance to project:

- There are no heritage items in the Kings Bay Stage Two precinct

2.3 CITY OF CANADA BAY PRCUTS STRATEGIES

There are a number of strategic documents that have been engaged by Council specifically for the PRCUTS. We have summarised these, and also graphically represented the key information in later chapters of this report.

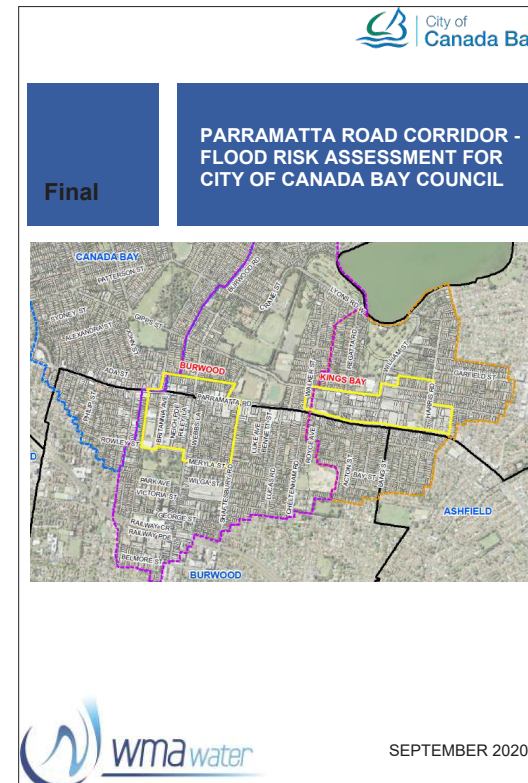


Sustainable Precinct Strategy

The Strategy provides strategies and mechanisms to deliver cost-effective and high environmental performance outcomes across all precincts.

Relevance to project:

The Stage One Sustainable Precincts Strategy was produced as an addendum to the Stage One Strategy. The addendum states therefore, that the proposed recommendations from Stage One, pertaining to sustainability considerations, are still deemed significant and should be considered and implemented in the subsequent stages of the project.



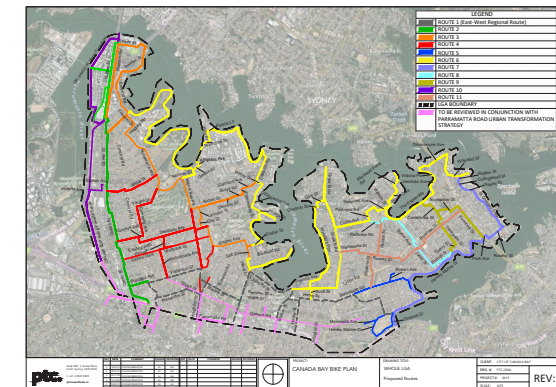
Parramatta Road Corridor Flood Risk Assessment

The assessment maps the flood risk across a number of potential scenarios for the Burwood and Kings Bay precincts.

Further flood assessment is intended, however had not been undertaken in time to inform the masterplan. Once this study is complete, the master plan will be coordinated and updated where needed.

Relevance to project:

- Key mapping of flood risk for each precinct has been graphically represented in later chapters.

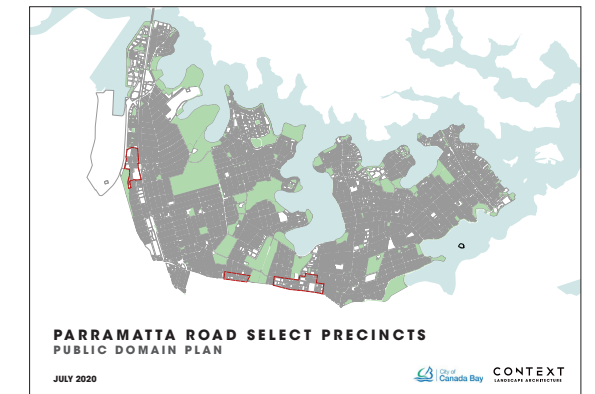


Canada Bay Bike Plan

The Draft Bike Plan proposes future extensions to the existing cycling grid within City of Canada Bay, linking key locations and providing a region-wide accessibility network.

Relevance to project:

- Future cycling link proposed on Airlington Street linking Queens Road to Parramatta Road and south towards Croydon.
- Future cycling link proposed on Walker Street linking Parramatta Road to Queens Road and north towards Renown Street, where it links with an extended path from St Lukes Oval to Five Dock.



Parramatta Road Select Precincts Public Domain Plan

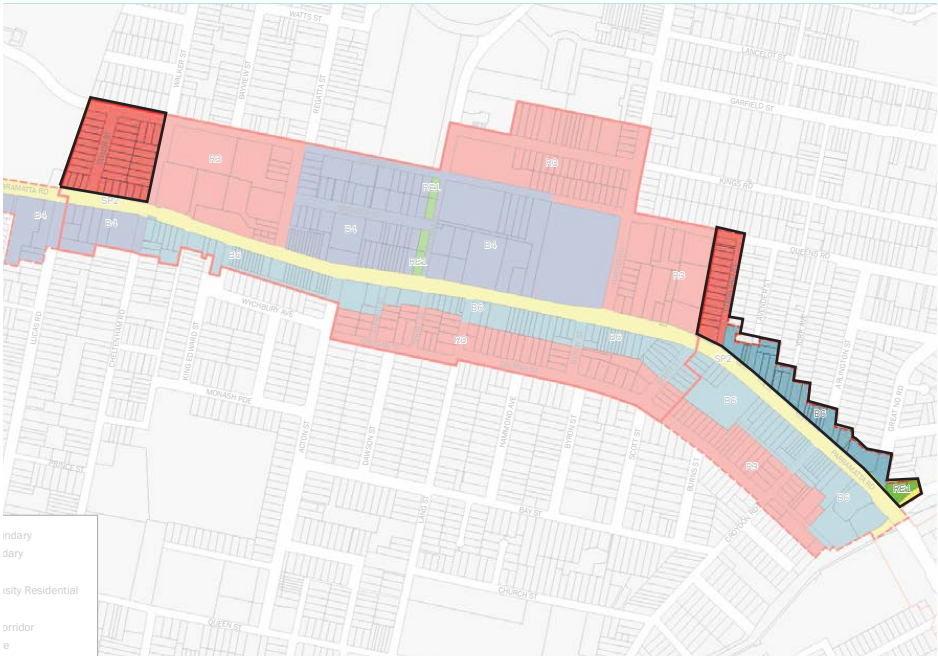
The Parramatta Road Public Domain Plan has driven the future public domain character in the precincts. In addition to the Stage 1 PDP, the masterplan needed to align with and inform the Stage 2 PDP.

Relevance to project:

- Future public domain character and specifications to be integrated into the Stage Two masterplan, creating cohesive wider precincts which respond to one another.
- Proposed street setbacks to be considered for re-application throughout Stage Two precincts.

2.4 PRCUTS DEVELOPMENT CONTROLS

Recommended Land Uses

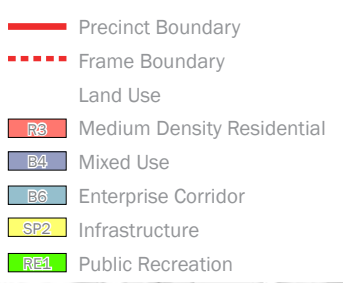


Recommended land uses within the Kings Bay Precinct vary from the study area's current controls, maintaining a B6 Enterprise Corridor zoning along Parramatta Road to its east, whilst converting all R2 zoned areas to rather R3 Medium Density Residential.

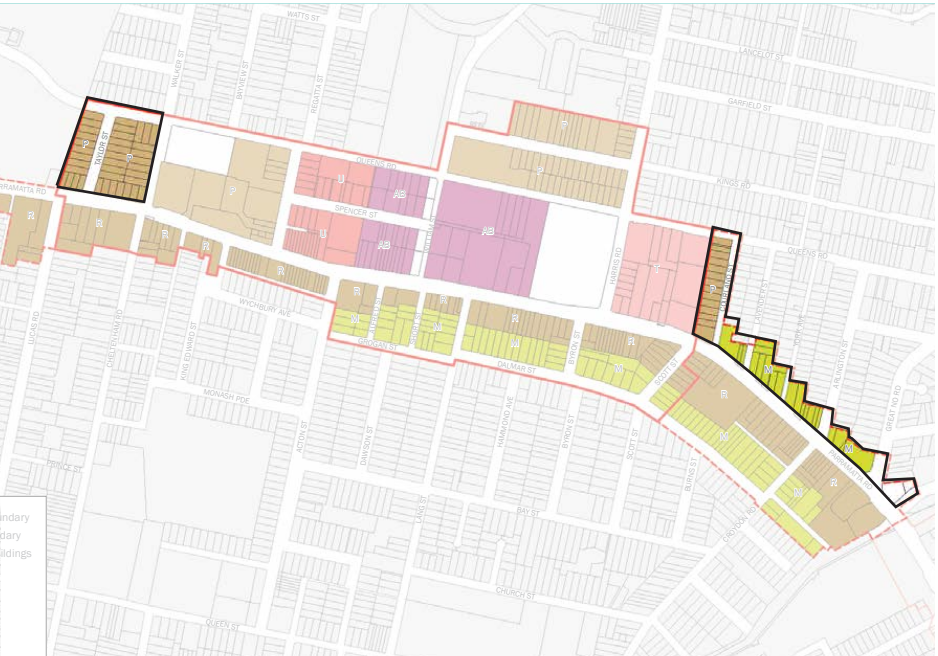
R3 Medium Density Residential areas maintain their residential character, but allow the introduction of multi-dwellings, town houses, and medium rise apartments.

A proposed local park zoned for RE1 Public Recreation is situated to the study area's south-east at its boundary extent.

As of April 2023, Canada Bay has adopted amendments to SEPP (Land Use Zones) 2022, in which business and environmental zones have been re-organised. Mixed-use zones are now represented by the MU 1 Mixed-Use zone, and the B6 Enterprise Corridor zone is now represented by the E3 Productivity Support zone.

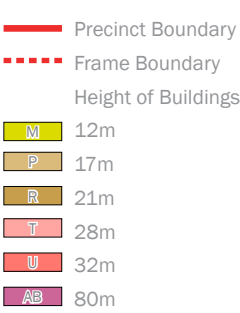


Recommended Building Heights

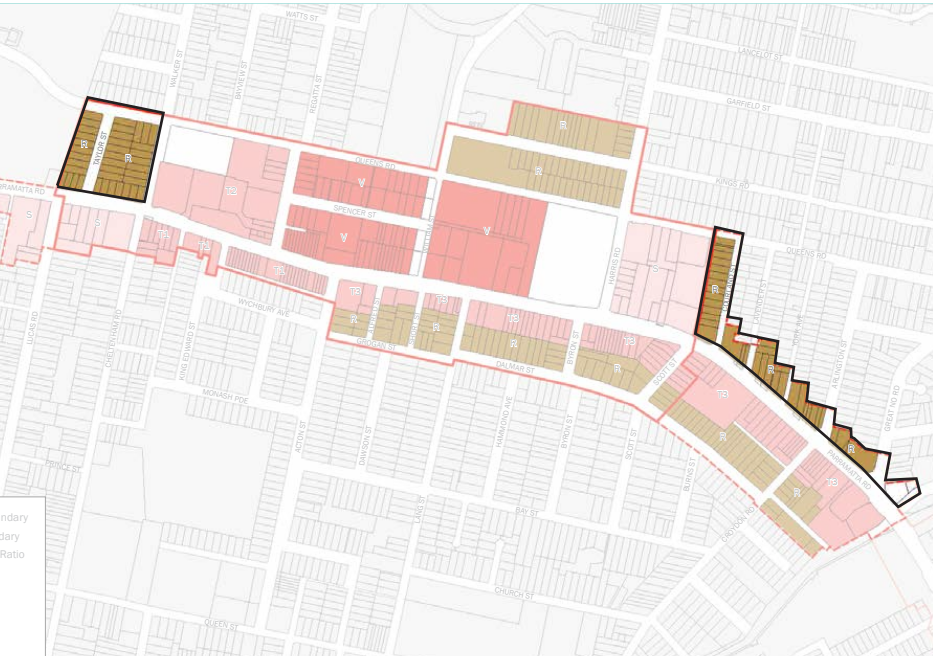


Maximum building heights throughout the Kings Bay Precinct aim to provide an appropriate transition between an uplifted town centre and the surrounding low-rise residential community.

Recommended heights within the study area reflect their peripheral location, with properties to the east maintaining a maximum building height of up to 17m, whilst development immediately adjacent to single-dwellings are afforded a 12m maximum height.

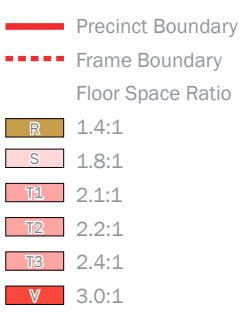


Recommended Densities



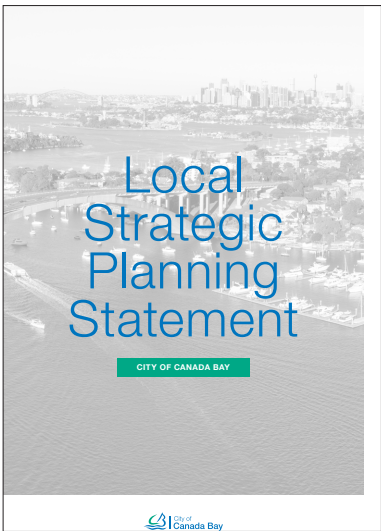
Recommended densities calculated as floor space ratio ensure that proposed development achieves a desirable size and scale which reflects their land use, and ensures that the maximum building height may be reached.

Densities throughout the study area achieve an FSR of 1.4:1 at either interface, allowing multi-story developments, but constraining development to reflect the surrounding primarily 0.5:1 FSR of the residential community.



2.5 CITY OF CANADA BAY POLICIES AND GUIDELINES

There are a number of Local Government Policies and guidelines that will be taken into consideration.



City of Canada Bay Local Strategic Planning Statement

The City of Canada Bay Local Strategic Planning Statement (LSPS) is the core strategic planning document for the City of Canada Bay. It will guide the character of the centres and neighbourhoods into the future.

The LSPS brings together and builds on planning work found in Council's other plans, studies and strategies such as the Local Environmental Plan (LEP), Development Control Plans (DCP) and Contributions Plans. The LSPS will be used to update key components of these plans.

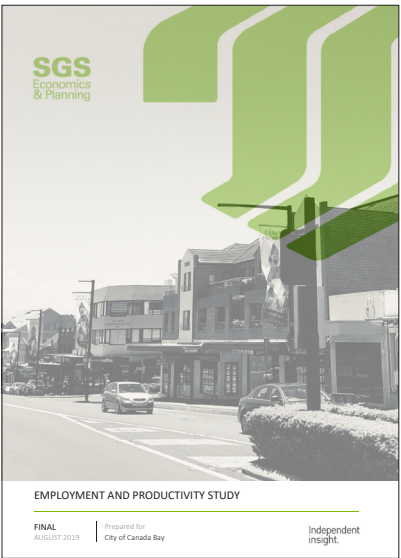
The Stage Two study area is identified as an Urban Renewal Area, which is achieved through investment in homes, jobs, transport, open space, and public amenity.

Relevance to project:

The PRCUTS precincts can respond to and strengthen the land use vision set by the LSPS. There are also a number of Priorities identified in the LSPS that PRCUTS can respond to:

- Priority 5 to provide housing supply, choice and affordability in key locations. The PRCUTS study is specifically highlighted as being able to directly respond to how the additional dwellings needed in the LGA can be provided.

- Action 5.5: Require a minimum of 5% of the Gross Floor Area of new development to be dedicated as affordable housing
- Action 7.8: Include a minimum lot size of 800sqm for boarding houses in the R2 density zones to improve the amenity of boarding houses and reduce their impact on these areas
- Action 10.1: Investigate and encourage new mixed use forms, larger format uses and urban support services on the ground floor of development with a frontage to Parramatta Road in the Kings Bay Precinct.
- Action 10.2: ensure that the future built form controls and the structure of street blocks facilitate:
 - + alternate access from a road, other than Parramatta Road
 - + double height ceilings for ground floor uses that front Parramatta Road
 - + rear lane low bay access for small truck and customer parking
 - + shared loading facilities for non residential uses



Employment and Productivity Study

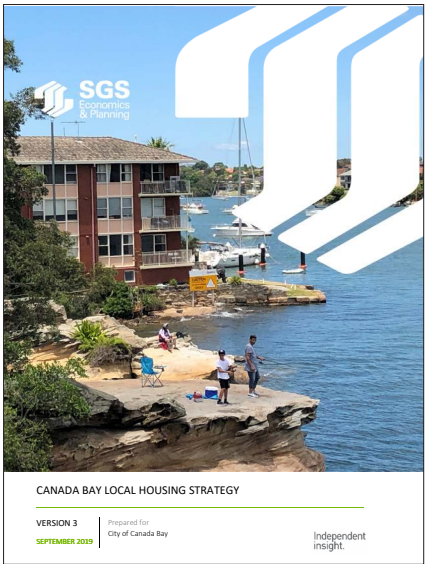
The study acknowledges that the Parramatta Road Corridor is undergoing significant change as part of the PRCUTS.

It makes recommendations that aim to ensure that sufficient and appropriately located retail floorspace is provided in the precinct's emerging centres to allow for future employment of local residents, to support emerging local businesses, and ensure that Canada Bay remains a productive region.

Relevance to project:

The study highlights issues present within the precinct which hinder its current productivity potential and aim to develop its local economy in future, in particular:

- Priority 9: Incorporate displaced industries in the B6 and IN1 zones within future mixed use and enterprise precincts along Parramatta Road as part of the PRCUTS
 - + Action 9a: Actively manage the inclusion of all types of permitted uses in the potential B4 (Mixed Use) and B6 (Enterprise Corridor) zones proposed in the PRCUTS
- Action 10c: Concentrate large bulky retail along Parramatta Road (including uses such as car show rooms)



City of Canada Bay Housing Strategy

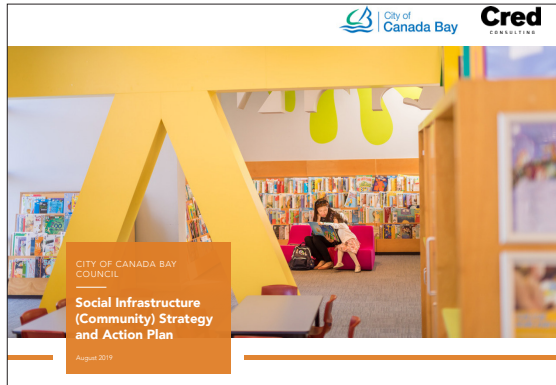
The Local Housing Strategy analyses the population, demographic and supply issues associated with the delivery and take up of housing in the LGA.

This assessment is required by City of Canada Bay Council to develop an understanding of what it could do to plan for and deliver optimal residential outcomes for its community.

Relevance to project:

The Strategy identifies a number of actions relevant to the PRCUTS:

- Large-scale urban renewal to deliver high density housing in the form of apartments as outlined under State Government plans
- Develop the Parramatta Road Urban Transformation Precincts as higher density apartment development areas in the short to medium-term.
- Housing diversity and choice to be further addressed by infill development around centres, based on planning controls that are feasible, to provide a wider range of housing forms whilst being respectful of local neighbourhood character



Social Infrastructure Community Strategy and Action Plan

Provides an audit of all community facilities located within our area and owned by Council, NGOs, private sector, and Government agencies.

These trends and principles should inform the future kinds of social infrastructure that is funded and developed within Canada Bay, as well as how they are delivered. The following principles should drive the design and delivery of future community facilities, services and programs to 2036:

1. Diverse and activated
2. Inclusive and equitable
3. Connected and co-located
4. Collaborative and shared
5. Multipurpose and future-proofed

Relevance to project:

The plan identifies key moves for the Five Dock/Canada Bay Catchment aimed at improving Community amenities including the inclusion of:

- Communal facilities within high density
- 400m2 community centre in town centre (near future metro)
- Increased seniors housing



Open Space and Recreation Strategy and Action Plan

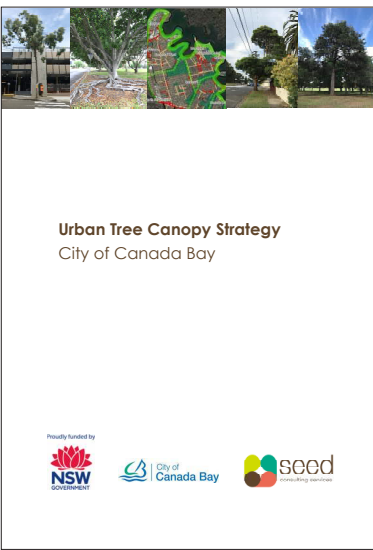
This report provides a Strategy and Action Plan for social infrastructure (open space and recreation) to 2019, 2026 and 2036. This work identifies our community's needs, to inform development of a deliverable action plan to inform Council's budgeting process as well as the operational and resourcing plans for open space and recreation facilities, programs and services.

The Strategy and Action Plan has been developed alongside a suite of other focus area strategies - community facilities, housing, economic productivity, biodiversity, traffic and transport - as part of a review of the City's planning framework. This review includes the development of the Local Strategic Planning Statement (LSPS), the land use strategy for the next 20 years and implementation through a revised Local Environmental Plan (LEP) and Development Control Plan (DCP).

Relevance to project:

The plan identifies key moves for the Five Dock-Canada Bay Catchment aimed at improving local open space provision including the inclusion of:

- Improved connections along Iron Cove Creek from Parramatta Road (2036)
- New open space in town centre (minimum 0.3ha) to support increased population including from future Metro station (2036)



Urban Tree Canopy Strategy

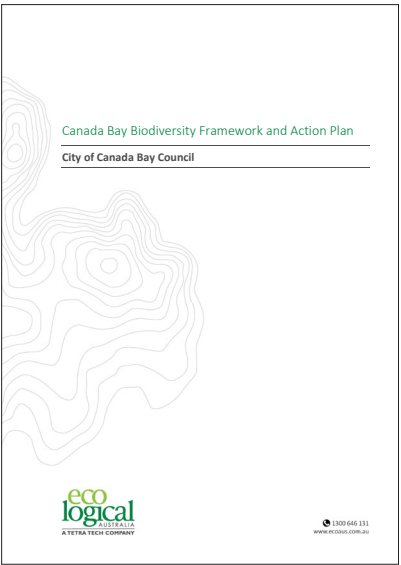
The purpose of this Urban Tree Canopy Strategy is to inform the development of the revised City of Canada Bay Local Environment Plan and to present Council's vision, priorities and actions to managing the urban forest.

Achieving the proposed increased canopy cover target will require extensive tree plantings on public and private spaces. If planned appropriately, such plantings can achieve multiple outcomes and maximise the benefits from trees. To identify broad priority planting locations, the following factors were integrated: canopy cover, potential plantable space (Sections 5.2 and 5.3), thermal heat mapping (Section 6.4), and location of proposed green grid opportunities, parks and streets.

Relevance to project:

Burwood is identified as medium priority

The strategy also looks at opportunities to interface with the Greater Sydney Green Grid.

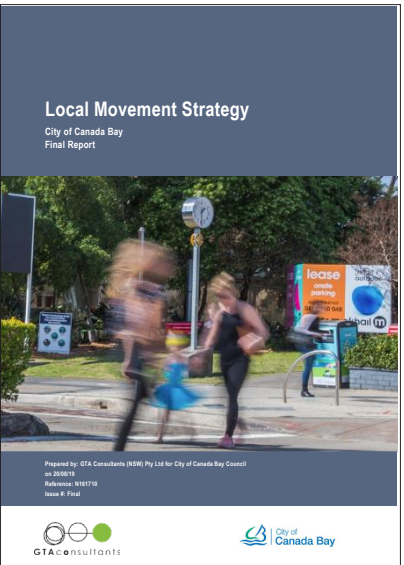


City of Canada Bay Biodiversity Framework

City of Canada Bay has developed this Biodiversity Framework and Action Plan to help to ensure that local ecosystem health including species and their genetic diversity survive in their natural habitat.

The Biodiversity Framework and Action Plan supports the Local Strategic Planning Statement which sets out the 20-year vision for land-use in the local area.

The plan is based upon six interconnected themes: native vegetation, urban waterways and foreshores, corridors and connectivity, public spaces, urban habitat and green infrastructure. This plan is supported by international, national, state and local policy that drive the development of a biodiversity plan at the local level. This Biodiversity Plan provides capacity to reinforce regional connections and enhance local corridors. It will allow for regional partnerships and is flexible enough to embrace any future infrastructure and development.



Local Movement Strategy

An overview of the existing transport, opportunities and constraints, future transport and land use trends and changes.

It also presents a series of actions per travel mode that support overarching strategic objectives across the Canada Bay Local Government Area (LGA). It provides a list of key future projects within and surrounding the PRCUTS precinct study areas including; WestConnex, Sydney Metro West.





3.0 ANALYSIS

03

“Kings Bay will be a new residential and mixed-use urban village on Parramatta Road, with an active main street and strong links to the open space network along Sydney Harbour.”

PRCUTS Stage Two Vision for the Kings Bay Precinct



3.1 STUDY AREA

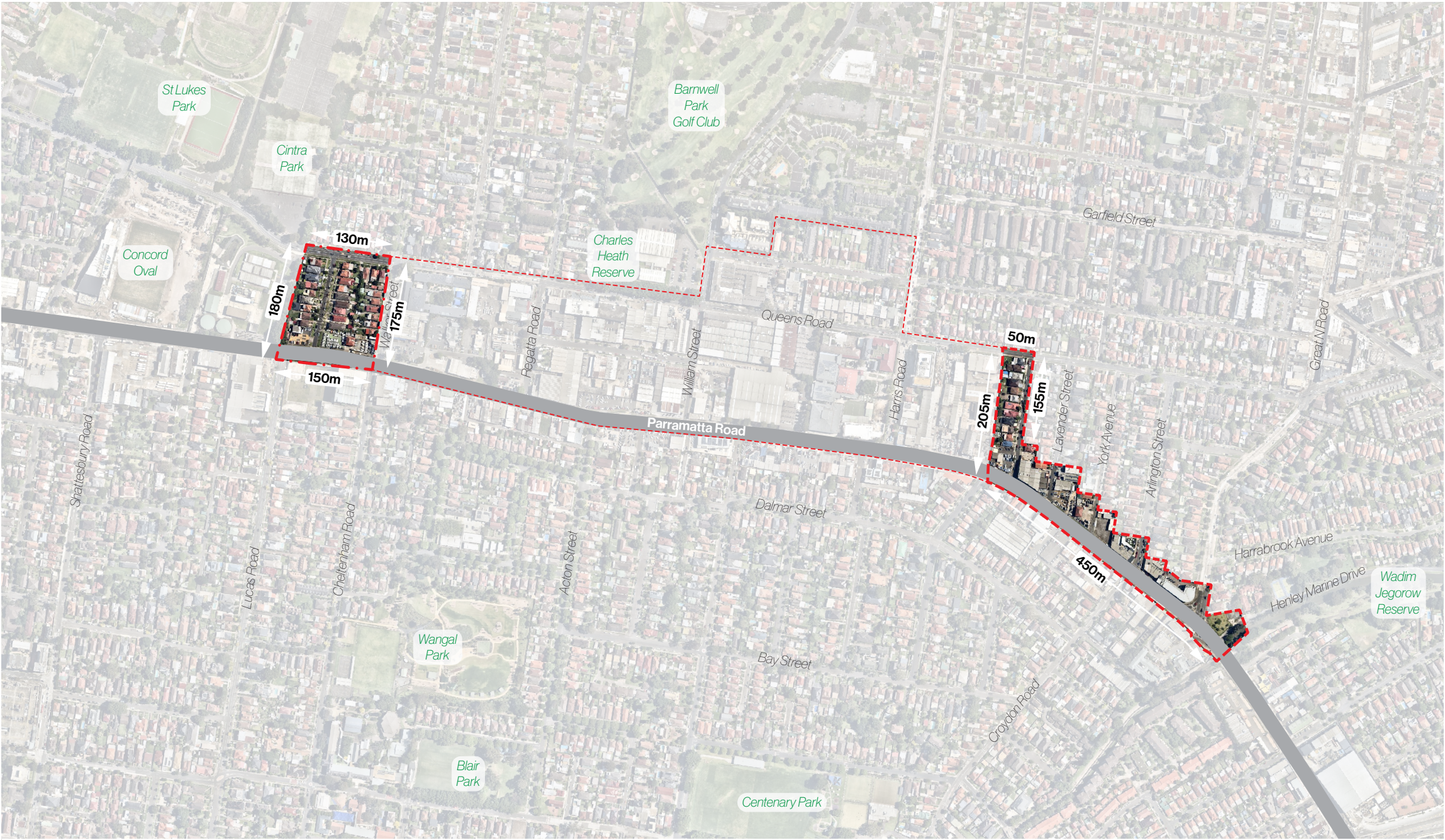
The Kings Bay Precinct consists of two small segments connected via Queens Road, located on either end of the PRCUTS Stage One study area.

The western precinct includes commercial premises along Parramatta Road and those premises receding northwards, as well as some residential dwellings located on Courland Street.

The majority of these commercial premises are related to the automotive industry, though the Illinois Hotel represents a local social establishment within the precinct boundary.

The eastern precinct extends from Courland Street to Henley Marine Drive primarily constituting commercial frontages along Parramatta Road.

Residential dwellings on Courland Street are low-rise, reflecting the overall character of the Kings Bay area, and much of the residential community immediately adjacent to the study area.



LEGEND

- Stage 2 Kings Bay Precinct East
- Stage 2 Kings Bay Precinct West
- Kings Bay Precinct

Parramatta Road

Scale 1: 7,500 @ A3
0m 30 60 150 300



3.2 LOCAL CHARACTER

Kings Bay Precinct West

Streetscape Character

The Kings Bay Precinct's west includes two minor local roads, Taylor Street which links Queens Road to Parramatta Road, and Walker Street which is no-through.

Taylor Street and Walker Street are largely residential in character, though Walker Street faces Lucas Gardens School to its east and features a mature canopy unlike Taylor Street.

Streets are not often utilised as through-roads, servicing primarily local traffic.

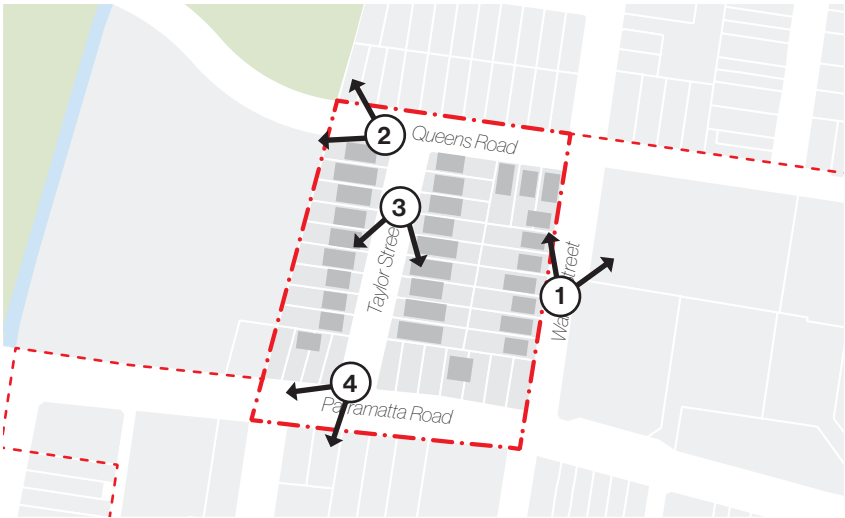
Access to Taylor Street via Parramatta Road is limited to easterly traffic. Walker Street features a landscaped road barrier which restricts vehicular access from Parramatta Road.



1. Existing mature trees provide canopy cover along Walker Street



2. Taylor Street interfaces Queens Road to its north, a local east-west arterial road.



3. Single residential dwellings with little canopy cover.



4. Taylor Street interfaces Parramatta Road to its south.

Kings Bay Precinct West

Built Form Character

Commercial premises comprise the precinct's entire southern interface except for a single western-most dwelling, with large open lots and setback buildings.

Commercial premises function as a buffer to Parramatta Road, reducing impact on interior residential dwellings.

Dwellings consist of single dwelling houses, with minor front setbacks to the street. A considerable planted verge mediates between the setback pavement and Walker Street, though street trees are sparse.



1. Single dwellings on Taylor Street feature minor front setbacks, with few street trees.



2. Commercial frontages to Parramatta Road.



3. Commercial frontages act as a buffer to Parramatta Road.



4. Single dwellings on Taylor Street face non-residential frontages to the east.

Kings Bay Precinct East

Streetscape Character

The eastern half of the Kings Bay Precinct runs parallel to Parramatta Road, including only the relevant gateways and intersections to interior local roads, as well as the primarily residential Courland Street.

These local roads differ greatly in typology to Parramatta Road, being single lane and servicing primarily local traffic.

They are unmarked, with street-parking on either side often allowing only a single lane of traffic. Furthermore, the lack of verge with a public domain consisting solely of a pavement leaves them without any public planting, relying on private front-porch planting instead.

This however does not apply to Airlington Street, which features a widened road and minimum two lanes of traffic, alongside a thin verge strip, though planting is still sparse.

At this interface, Parramatta Road is variously afforded a landscaped verge between Lavender Street and York Avenue due to the lack of active frontages, though elsewhere this is negated, creating a considerably wide footpath.



1. Great North Road, a secondary north-south thoroughfare



2. Lavender Street links Parramatta Road to Queens Road



3. Parramatta Road intersection with Airlington Street.



4. Henley Marine Drive links Parramatta Road to Ramsay Street.

Kings Bay Precinct East

Built Form Character

Consisting of commercial frontages along Parramatta Road, and a number of residential dwellings on Courland Street, the Kings Bay Precinct's east concerns primarily the interface between the interior residential community, and Parramatta Road congestion.

Built form along Parramatta Road is either afforded a considerable setback, or a large building height which acts as a buffer.

Towards the precinct's south, large buildings occupy prominent intersections with the Great N Road and Arlington Street, creating an effective street-wall.

Commercial properties with a considerable setback however break this consistent street-wall, reducing the effectiveness of this buffer and allowing greater visual permeability to the residential interior.

Courland Street features the only such residential dwellings located within the study area, though their primarily single storey height and proximity to future development areas is problematic for their current building typology.



1. Single dwellings are low-rise in character, and feature minor setbacks.



2. Commercial frontages to Parramatta Road .



3. Single dwellings interface adjacent commercial premises.



4. Parramatta Road features an inconsistent public domain, with pavement awnings sparsely located.

3.3 ANALYSIS

Road Hierarchy

The Kings Bay Precinct consists of a series of local residential roads which link Parramatta Road north to Queens Road.

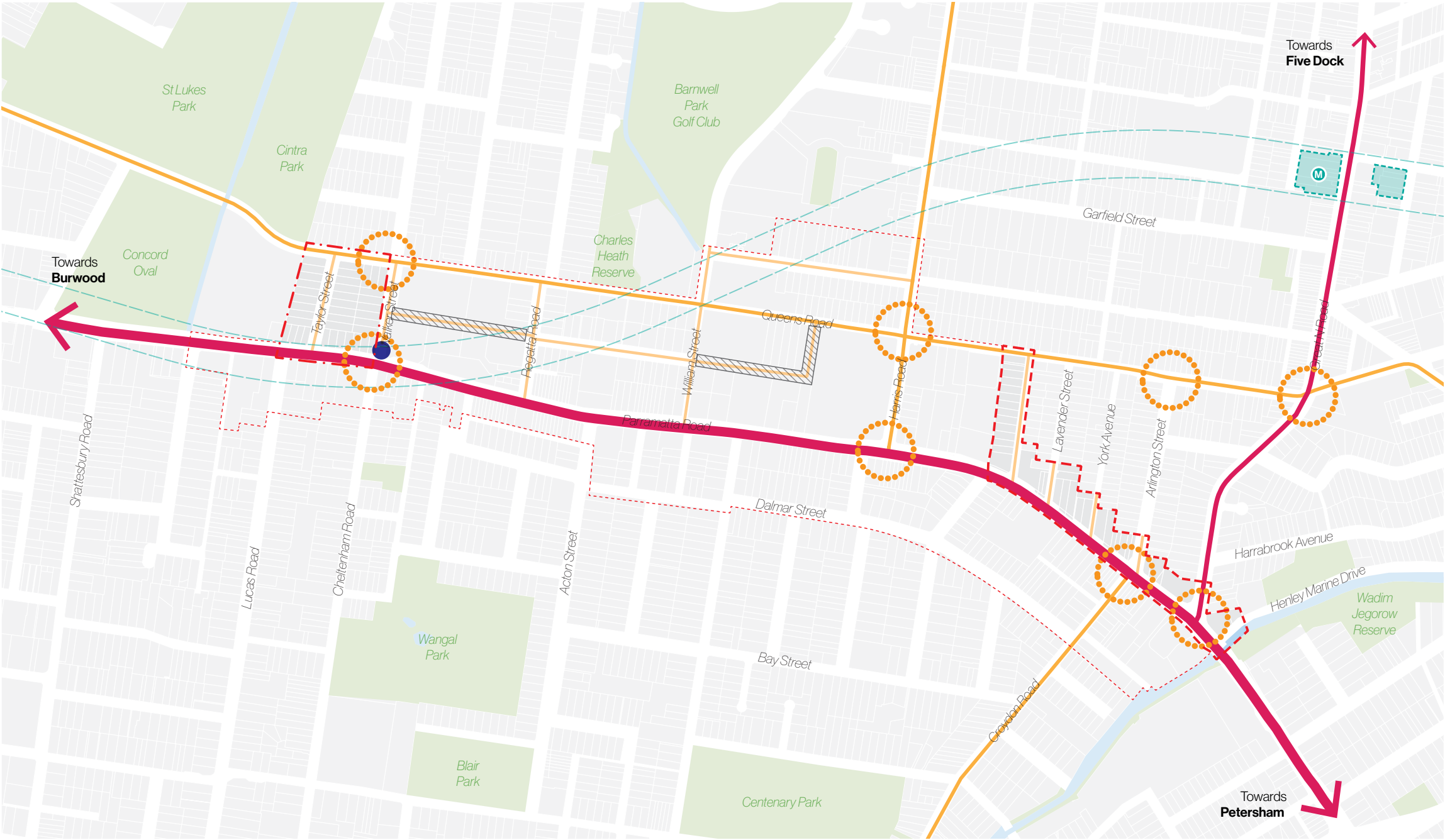
Through-traffic is concentrated on Harris Road and Great North Road, which link the precinct north to the suburbs of Five Dock and Wareemba.

Local residential roads within the stage two study area receive little through-traffic, with the exception of Arlington Street, which is an extension of Concord Road to its south.

Signalled intersections mediate traffic at Walker Street, Harris Road, Arlington Street, and Great North Road to both Parramatta Road and Queens Road.

Vehicular access to Walker Street via Parramatta Road is restricted by a landscaped road barrier, allowing only cycle and foot traffic.

The existing road network of the Kings Bay Precinct is proposed to be extended as part of the Stage One masterplan, including an east-west link between Walker Street and William Street.



LEGEND

- | | | | |
|---------------------------------|----------------|-----------------------------------|---------------------|
| Stage 2 Kings Bay Precinct East | Motorway | Signalled Intersection | Proposed Metro Site |
| Stage 2 Kings Bay Precinct West | Arterial Road | Proposed Road Reserve (Stage One) | Metro Tunnel |
| Kings Bay Precinct | Collector Road | Vehicular Access Restricted | |
| Open Space | Local Road | | |

Scale 1: 7,500 @ A3
0m 30 60 150 300

Public and Active Transport

The existing public transport network includes bus routes along major roads, and future Metro stations.

The 530, 461, and 415 bus services link the study area's western half to Five Dock town centre and extend westwards towards Strathfield town centre.

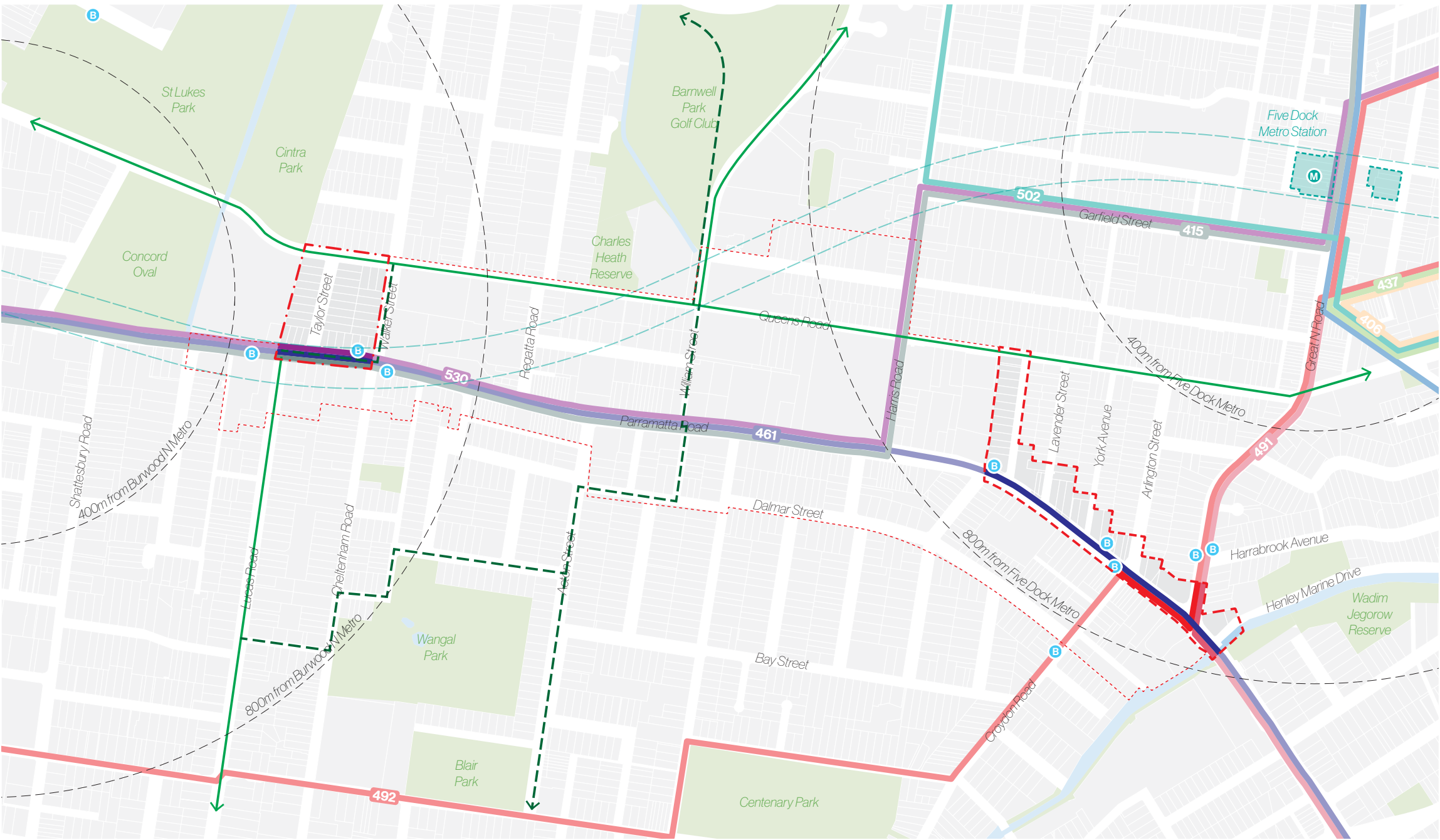
The introduction of the Five Dock Metro Station to the site's north-east will significantly improve accessibility to the Kings Bay Precinct which currently does not have significant linkages to the metropolitan area at large.

The Kings Bay Precinct is within walking distance of Burwood North Metro Station and Five Dock Metro Station, which will improve future connectivity.

The 530 and 415 bus routes will also provide connectivity to nearby Metro Stations.

Cycling routes along Queens Road connect the Kings Bay Precinct to the Burwood North Precinct, extending south along Lucas Road.

Alternative cycling links as proposed in the PRCUTS Planning and Design Guidelines 2016 will improve this southerly link with an additional Acton Street link extending through the precinct's proposed town centre.



LEGEND

- | | | | |
|---------------------------------|------------------|---------------------|---------------------|
| Stage 2 Kings Bay Precinct East | Bus Route | Existing Cycle Link | Proposed Metro Site |
| Stage 2 Kings Bay Precinct West | Bus Stop | Proposed Cycle Link | Metro Tunnel |
| Kings Bay Precinct | Bus Route Number | | |
| Open Space | | | |

Scale 1:7,500 @ A3
0m 30 60 150 300

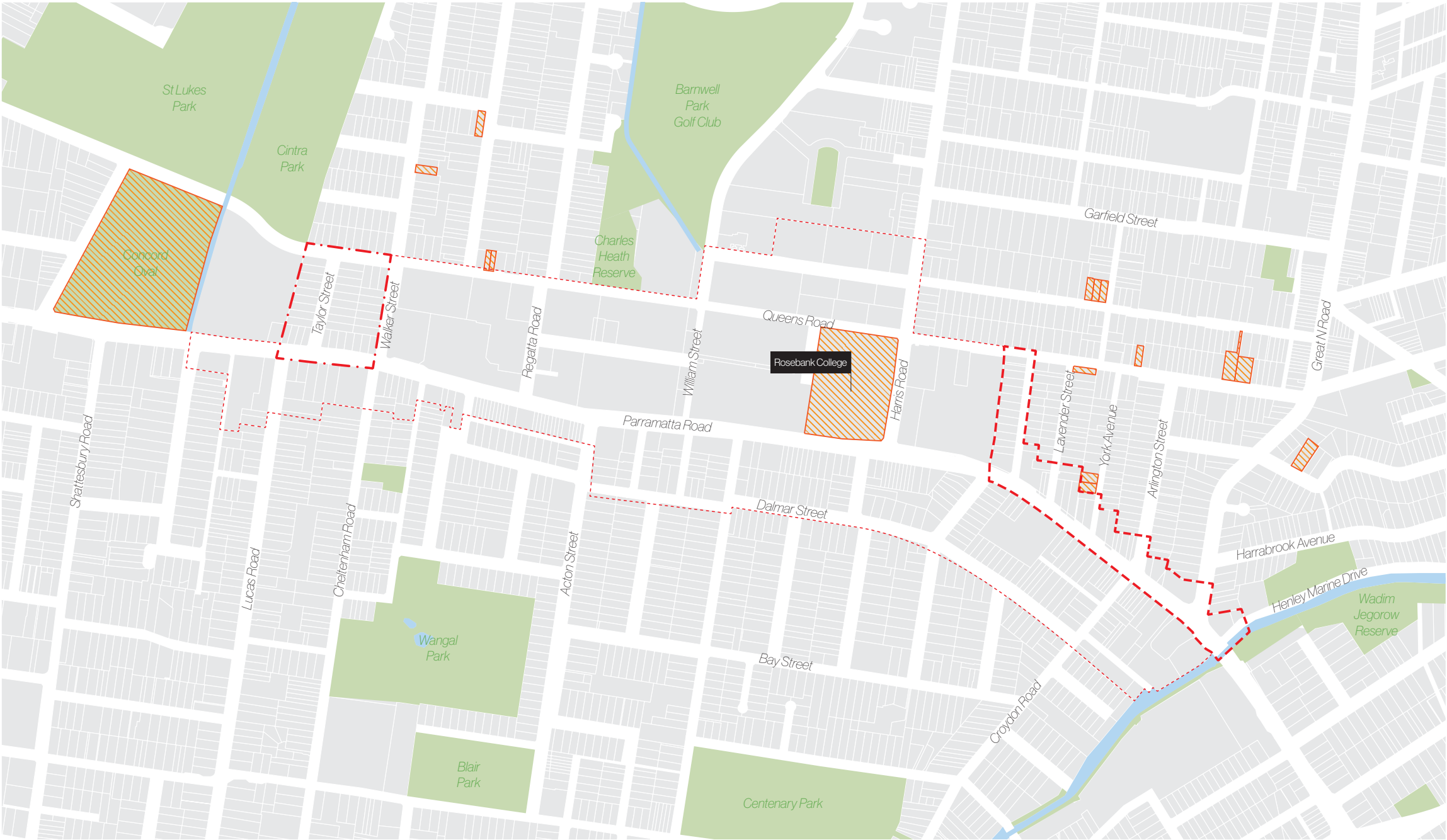
Heritage

There are no local or state heritage listed items within the boundary of this study.

However, there are heritage items located near the study area that will require consideration of built form in the proposed master plan.

There are two residential dwellings with heritage listings to the immediate north of the study area on York Avenue. The proximity of these to the site boundary will require careful design of the proposed envelopes to ensure ADG compliance.

Further to the west, Rosebank College is heritage listed. This is a significant distance from the study area, and is west of any envelopes that this study will be proposing.



LEGEND

	Stage 2 Kings Bay Precinct East		Heritage Item
	Stage 2 Kings Bay Precinct West		
	Kings Bay Precinct		
	Open Space		

Scale 1: 7,500 @ A3

0m 30 60 150 300

Topography and Ecology

There is an overall topographic decline outwards from the Kings Bay Precinct towards Iron Cove Creek and the westerly waterway respectively.

This decline differs between the two halves of the precinct, the western half experiencing an overall approximate decline of 8m, while the eastern half experiences approximately 24m, though these occur at a similar steepness.

The topographic high point is to be found on Courland Street, which then descends west-east down Parramatta Road.

Due to the primarily commercial/industrial land use found throughout much of the Kings Bay Precinct stage 1 study area, existing vegetation is sparse and found only in large pockets within the Rosebank College and Lucas Gardens School grounds.

Street planting on Taylor Street and in the precinct's eastern local roads consists of smaller trees found in private property due to these streets lack of verge and reduced setback.



LEGEND

- Stage 2 Kings Bay Precinct East
- Stage 2 Kings Bay Precinct West
- Kings Bay Precinct
- Open Space

Vegetation Canopy

- Contours 2m
- Topographic Maximum
- Site Slope

- Proposed Metro Site
- Metro Tunnel

Scale 1: 7,500 @ A3
0m 30 60 150 300

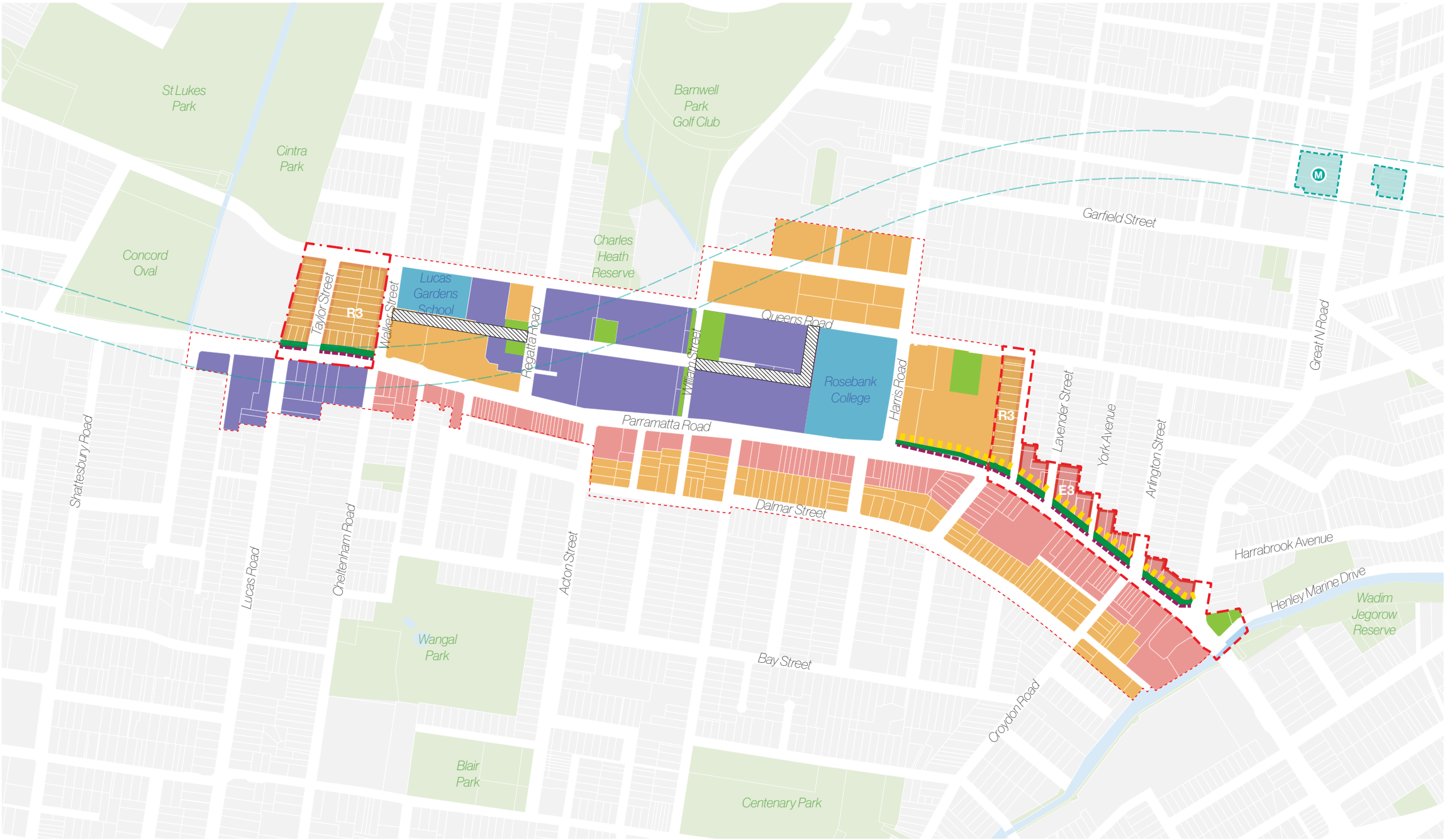
3.4 PRCUTS STRATEGIC CONTEXT

Land Use and Activation

The proposed improvements to the precinct’s future land use mix and potential ground-level activation as outlined in the *PRCUTS Planning and Design Guidelines 2016* has informed our understanding of the site and master plan approach:

- Rezone western precinct from R2 and B6 into R3 Medium Density Residential, allowing for a greater diversity of housing options.
- Maintain E3 Productivity Support zoning along the study area’s eastern precinct, except for development along Courland Street’s western interface which is to be rezoned as R3 Medium Density Residential.
- Green Edge Setbacks to be applied along Parramatta Road
- Commercial frontages to be maintained, or introduced where applicable along the entire Parramatta Road interface.

As of April 2023, Canada Bay has adopted amendments to SEPP (Land Use Zones) 2022, in which business and environmental zones have been re-organised. Mixed-use zones are now represented by the MU 1 Mixed-Use zone, and the B6 Enterprise Corridor zone is now represented by the E3 Productivity Support zone.



LEGEND

- Stage 2 Kings Bay Precinct East
- Stage 2 Kings Bay Precinct West
- Kings Bay Precinct
- Open Space

PRCUTS Proposed Zoning

- R3 Medium Density Residential
- MU1 Mixed Use
- E3 Productivity Support

- Education
- Proposed Open Space

- Proposed Road Reserve
- Variable TfNSW Setback
- Proposed Metro Site
- Metro Tunnel

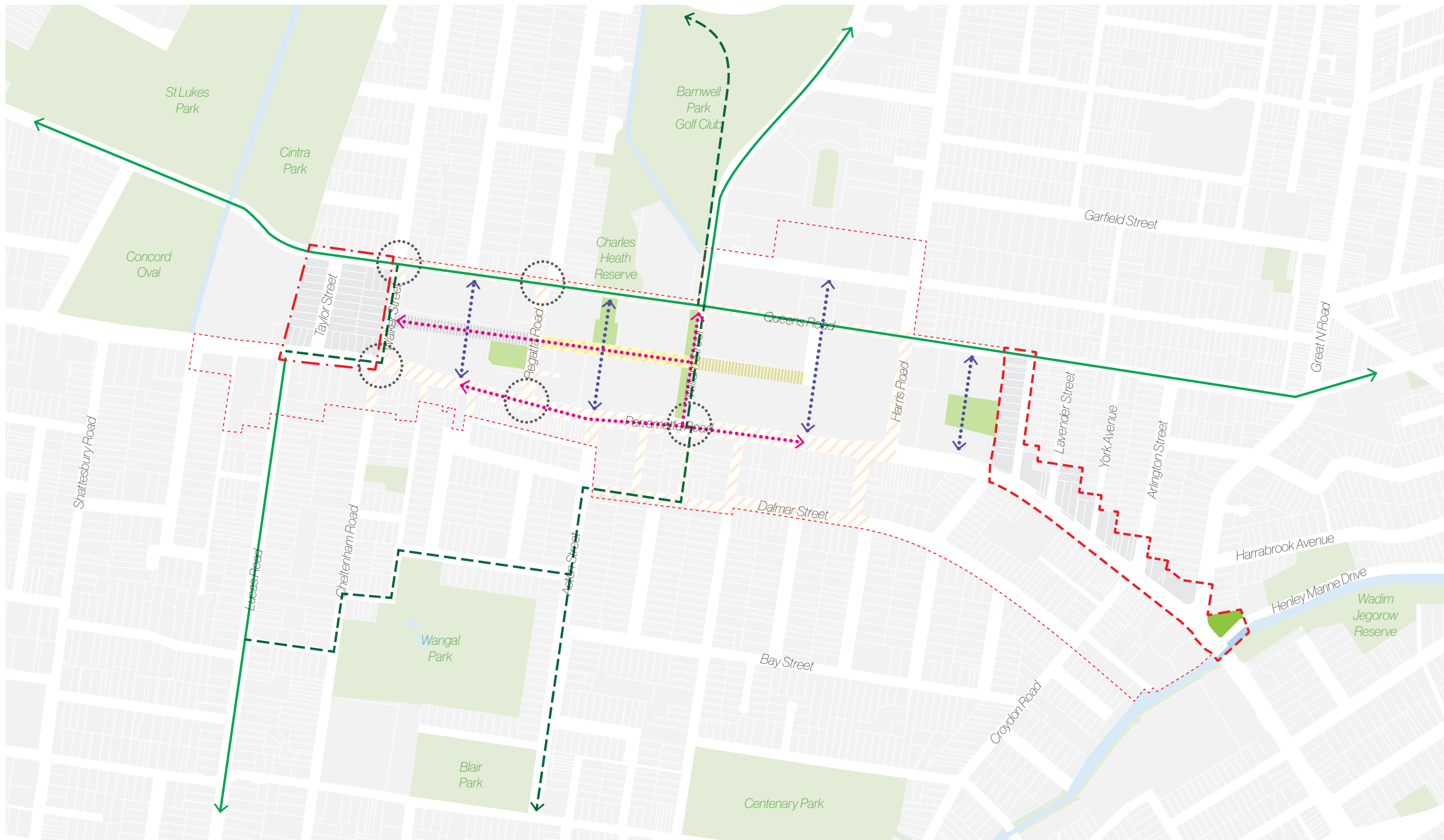
- Green Edge Setback
- Commercial Frontage

Scale 1: 7,500 @ A3
0m 30 60 150 300

Access and Connectivity

The proposed improvements to the precinct's public domain, potential linkages, and integration of high pedestrian activity zones as outlined in the *PRCUTS Planning and Design Guidelines 2016* has informed our understanding of the site and master plan approach:

- Additional open space to be located on Henley Marine Drive at the undeveloped lot facing Iron Cove Creek, extending the green network which runs along the creek East-West and providing a gateway into this residential local road.
- Cycle Link to be extended from Queens Road through to Lucas Road via Walker Street and Parramatta Road, with potential upgrades to these intersections including additional cycling infrastructure.
- Potential intersection upgrades to either side of Walker Street



LEGEND

- Stage 2 Kings Bay Precinct East
- Stage 2 Kings Bay Precinct West
- Kings Bay Precinct
- Open Space

- High Pedestrian Activity Zone (PRCUTS)
- Places for People (PRCUTS)
- PRCUTS Proposed Open Space
- PRCUTS Proposed Vehicular Link

- Existing Cycle Link
- PRCUTS Proposed Cycle Link
- PRCUTS Through-Site Link
- PRCUTS Prioritised Walking Link

- PRCUTS Potential Intersection Upgrade

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0m 30 60 150 300

Building Heights

The proposed improvements to the precinct’s future desired maximum building heights as outlined in the *PRCUTS Planning and Design Guidelines 2016* and the exhibited Stage One Kings Bay Precinct Master Plan has informed our understanding of the site and master plan approach:

- Building height within the Stage One Kings Bay Precinct study area reaches a proposed maximum of 79m along Queens Road.
- Surrounding peripheral built form provides a transition between this activity core and the low-density residential community to the precinct’s north.
- Residential dwellings on Taylor Street and Walker Street are afforded a 17m maximum building height, lacking otherwise immediately adjacent low-scale development with commercial interfaces on either side.
- Development with a residential interface within the precinct’s east is limited to 12m in building height, to ensure that the area’s current land use and typology is maintained.
- Dwellings on Courland Street are afforded a 17m maximum building height in contrast to adjacent 28m development.



LEGEND

- Stage 2 Kings Bay Precinct East
- Stage 2 Kings Bay Precinct West
- Kings Bay Precinct
- Open Space

- PRCUTS Control 2016
- PRCUTS Stage One MP 2022
- 2.5m
- 8.5m

- 12m
- 17m
- 19m
- 20m

- 21m
- 22m
- 28m
- 32m

- 35m
- 42m
- 45m
- 67m

- 73m
- 79m

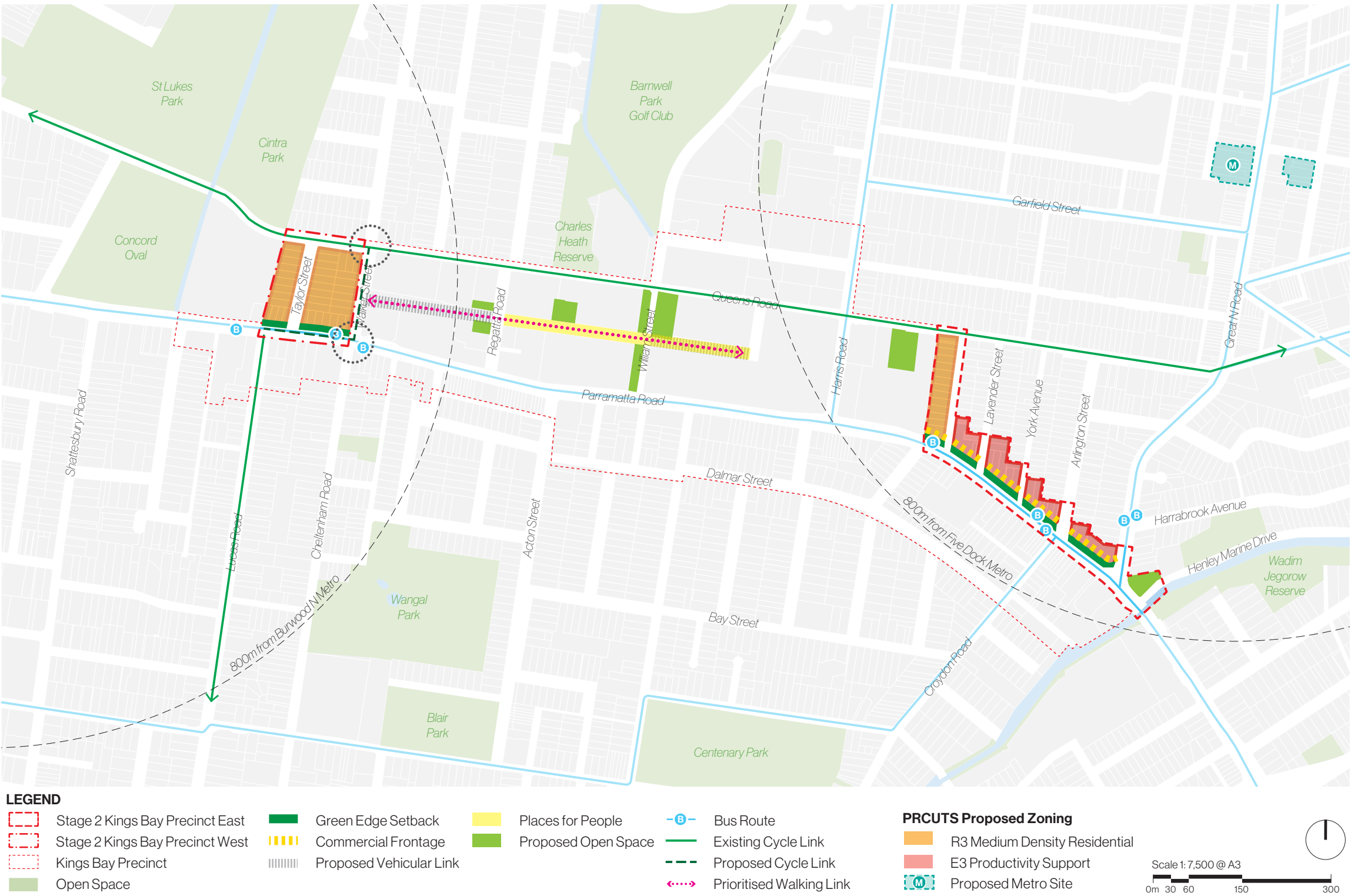
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3.5 OPPORTUNITIES

Opportunities

In reference to existing PRCUTS strategies, contextual considerations, literature review, and integration with the Stage 1 master plan, a number of opportunities identified within the precinct include:

- Integrate commercial premises on Parramatta Road into Stage 1 Mixed Use activity precinct connecting via Parramatta Road.
- Respond to proposed vehicular link towards western Walker Street frontage, with a supporting public domain that ensures a protected residential interface.
- Adopt green edge setback along Parramatta Road to ensure an active and pedestrian friendly interface.
- Locate higher density and high-rise built form along the Parramatta Road interface.
- Accommodate for proposed cycle links via Walker Street from Queens Road to Lucas Road to aid general local accessibility and to increase pedestrian use of the precinct.
- Maintain E3 Productivity Support for stand-alone commercial premises located between Courland Street and Iron Cove Creek.
- Adopt green edge setback along Parramatta Road to ensure an active and pedestrian friendly interface.
- Introduce additional open space towards the south of the precinct at Henley Marine Drive, developing under-utilised space and continuing the Iron Cove Creek network of green spaces.

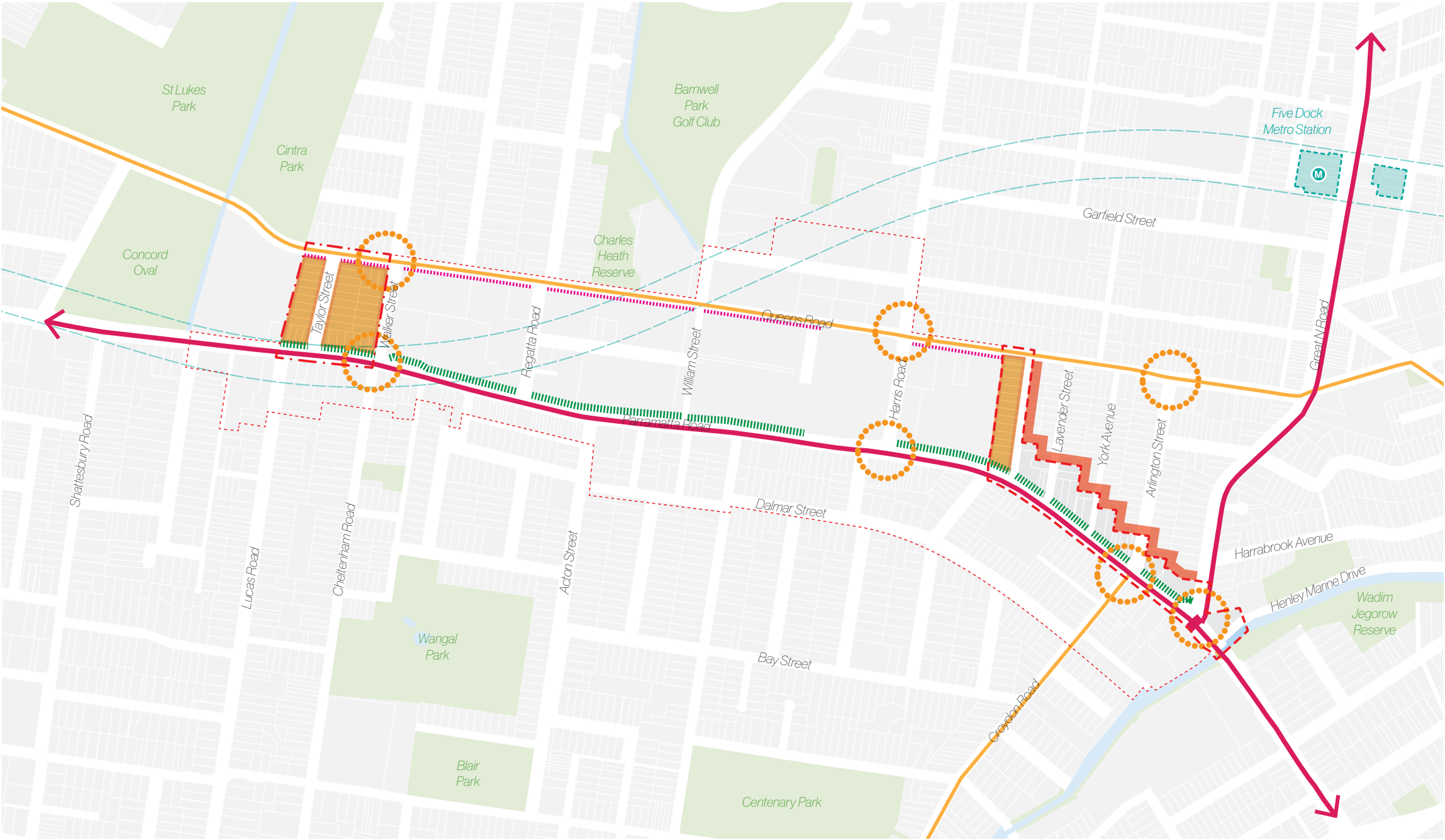


3.6 CONSTRAINTS

Constraints

In reference to existing PRCUTS strategies, contextual considerations, literature review, and integration with the Stage 1 master plan, a number of constraints to development identified within the precinct include:

- One-way access to Taylor Street via Parramatta Road due to the absence of a signalised intersection or turning point restricts vehicular movement in an easterly direction.
- Westerly directed access to Taylor Street limited to Queens Road, though Walker Street may be accessed from its north and southern intersections.
- Walker Street disjointed by landscaped roadblock, limiting connection to Queens Road from Parramatta Road.
- West precinct zoned entirely for residential development, requiring mediation against Parramatta Road.
- Proposed 6m landscape buffer along Parramatta Road interface.
- Proposed 3m buffer along northern Queens Road interface.
- East precinct zoned primarily for business use, abutting R2 Low Density Residential land along its northern interface.
- Croydon Road intersection with Parramatta Road introduces high vehicular congestion at Airlington Road intersection.
- Great North Road intersection with Parramatta Road introduces high vehicular congestion, limiting pedestrian access between proposed open space and rest of the precinct.



LEGEND

- Stage 2 Kings Bay Precinct East
- Stage 2 Kings Bay Precinct West
- Kings Bay Precinct
- Open Space

- Arterial Road
- Collector Road
- Signalised Intersection
- Residential Interface

- 6m Setback
- 3m Setback
- Proposed Metro Site
- Metro Tunnel

PRCUTS Proposed Zoning

- R3 Medium Density Residential

Scale 1: 7,500 @ A3
0m 30 60 150 300

[Redacted]





4.0 DESIGN PRINCIPLES

04

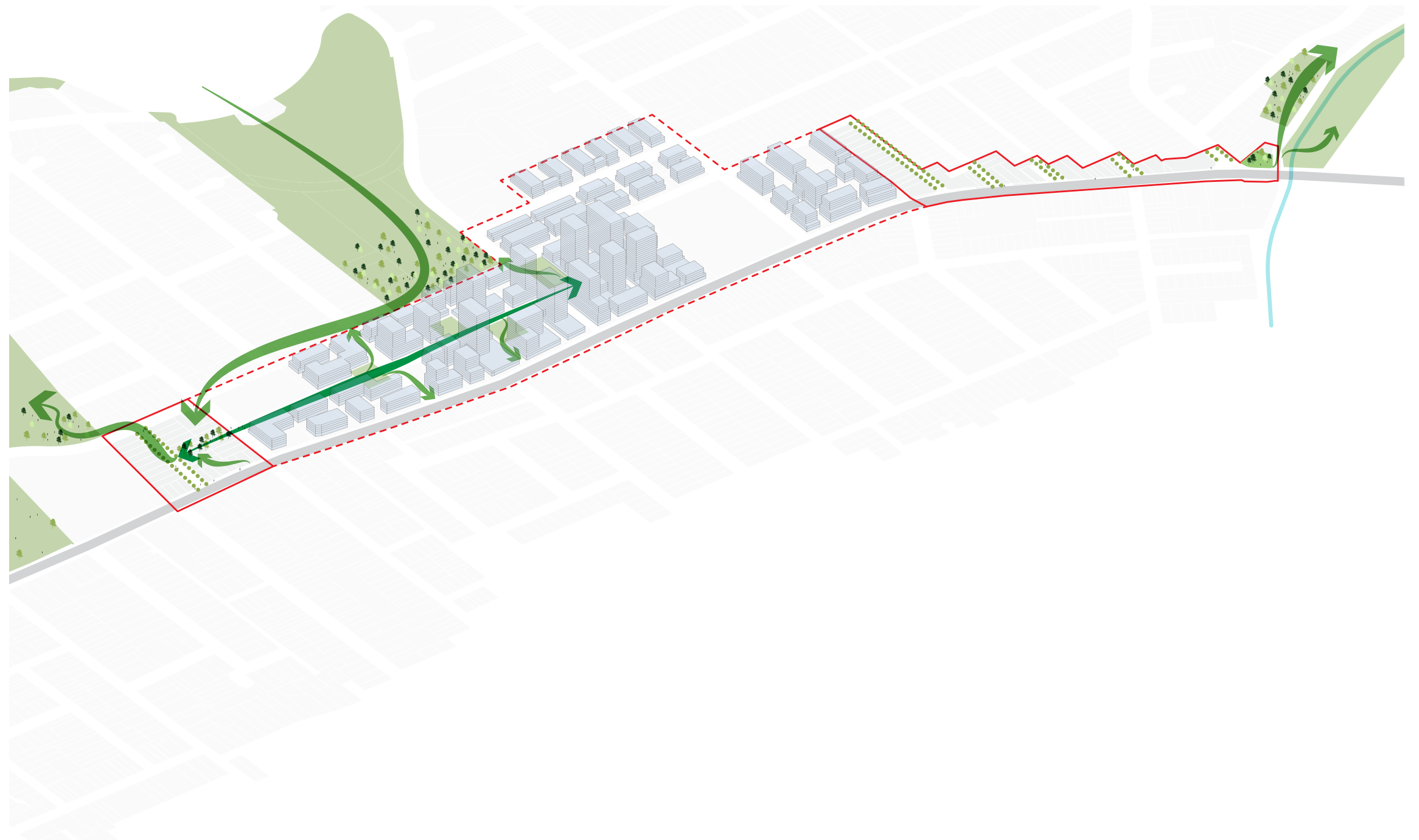
4.1 DESIGN PRINCIPLES

01/

Extend the Public Domain

Extend the Spencer Street connection into Stage 2, to concentrate pedestrian activity away from major traffic routes.

The extension of the east-west pedestrian link that extends the Spencer Street Eastern Extension enhances the overall quality of the public domain experience.

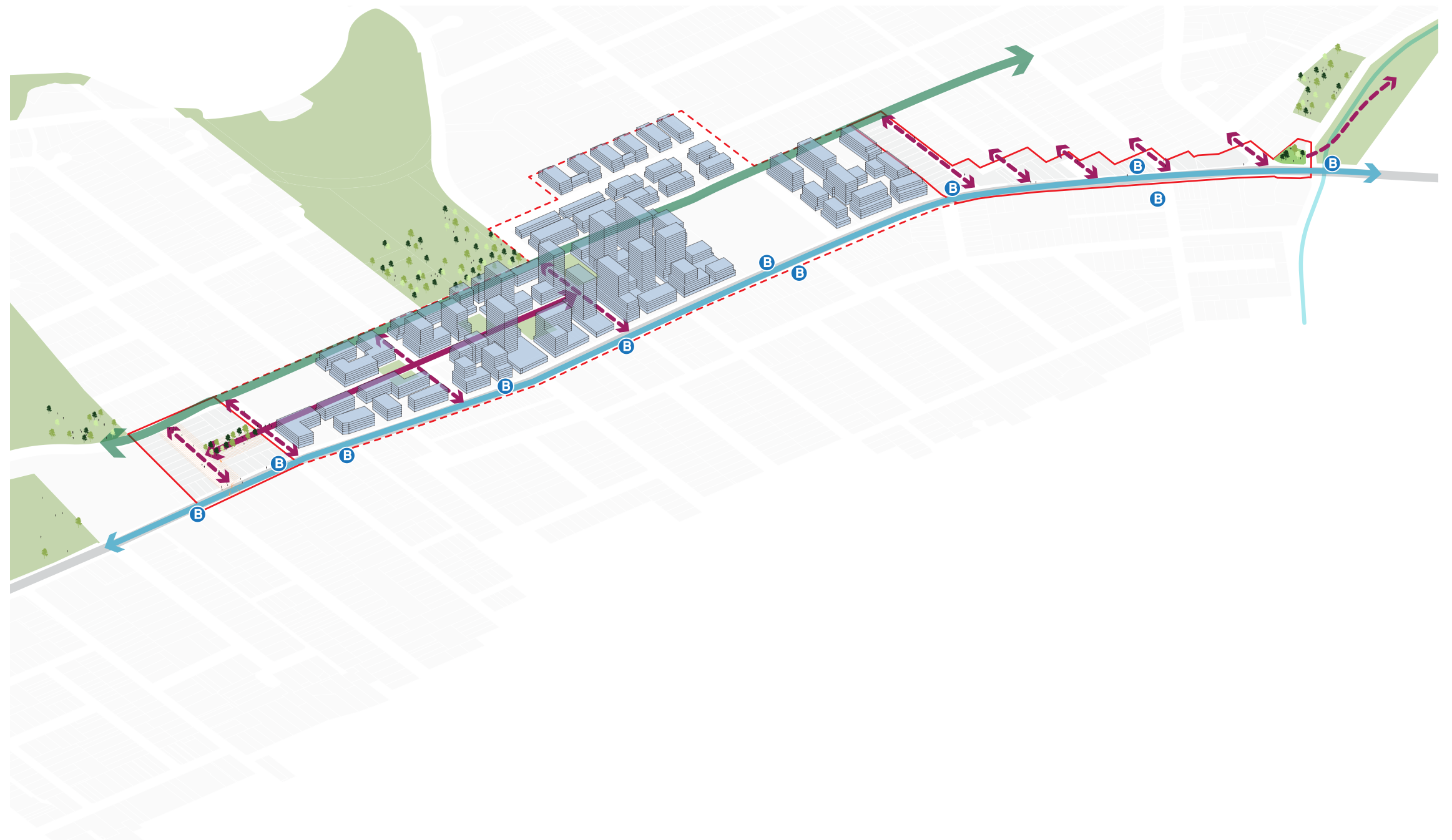


02/

Improve Activation along Parramatta Road

Commercial frontages along Parramatta Road will ensure a consistent active facade is expressed throughout the Kings Bay precinct, and the wider corridor.

Residential floorspace is concentrated away from Parramatta Road with north-south connections providing an access network to the main corridor.



03/

Taper heights to respond to future surrounding context

Proposed built form respond to the precinct's immediate context, with a two storey street-wall and height concentrated centrally.

Low-rise residences to the precinct's north are up to two storeys in height, which has been reflected along the Queens Road Interface.

This two-storey street-wall is extended along Walker Street, reducing in height towards the adjacent Lucas Gardens School.



04/

Improve Public Domain

New street typologies will compliment the built form proposed in the master plan to ensure improved pedestrian connections and amenity.

The continuation of the Spencer Street connection will ensure a consistent public domain approach between stage 1 and stage 2, and provide increased amenity, canopy cover and streetscapes for the new master plans.







5.0 MASTER PLAN

05

5.1 KINGS BAY PRECINCT VISION

Kings Bay is envisaged as a new residential urban village with a Parramatta Road address. It will have a dense network of streets and an identity built on its proximity to Sydney Harbour.

- Parramatta Road Corridor Urban Transformation Strategy, UrbanGrowth, 2016

Upon entering the Precinct and crossing over Iron Cove Creek, a proposed open space will serve as a pocket park and a welcoming entry point for the community of Kings Bay Precinct. This park will be an integral part of the open-space network that stretches along Iron Cove Creek, connecting to Timbrell Park and Parramatta River beyond.

The rejuvenated streetscape will extend northwest along Parramatta Road, contributing to the regeneration of the Parramatta Road area.

Within the Precinct, streets will feature a mix of low to medium-density residential buildings and mixed-use buildings with retail spaces fronting Parramatta Road. Residential development will be thoughtfully designed to harmonise with the character of the heritage homes present to the north and existing residential neighbourhoods, as well as the open spaces, sports facilities and schools situated across the precinct.

The built form will generally taper down towards the north, gradually transitioning into the adjoining lower-scale residential areas, ensuring a seamless integration with the surrounding environment.

The introduction of new open spaces and through-site links within the Precinct will establish a permeable network of walkable paths, reinforcing the links to neighbouring open space and connecting seamlessly with PRCUTS Stage 1. These enhancements will improve connectivity and actively encourage pedestrian traffic.





5.2 LOT AMALGAMATION

Lot amalgamation is proposed in consideration of existing conditions, to achieve desired public domain and urban design outcomes.

Amalgamation of lots will ensure that the desired built form controls outlined in *PRCUTS Planning and Design Guidelines 2016* may be achieved. Larger lots allow a greater degree of flexibility required to achieve desired FSR and maximum building height allowances.

Lot No.	FSR (n:1)	PRCUTS FSR (n:1)
Lot A		
A1	1.9	1.4
A2	1.8	1.4
Lot B		
B1	1.9	1.4
B2	2.0	1.4
Lot C		
C1	1.5	1.4
C2	1.6	1.4
C3	1.8	1.4
Lot D		
D1	1.6	1.4
Lot E		
E1	1.5	1.4
Lot F		
F1	1.4	1.4
Lot G		
G1	1.5	1.4

Kings Bay Precinct West



LEGEND

- Stage 2 Study Area
- Kings Bay Precinct
- Amalgamation Boundary
- Open Space
- Heritage Item

- Proposed Building
- Proposed Road Reserve
- Required / Not to be dedicated
- Required / To be dedicated

- TfNSW Road Widening + 6m Public Domain to be dedicated to Council

Kings Bay Precinct East



Scale 1: 5,000 @ A3

0m 20 40 100 200

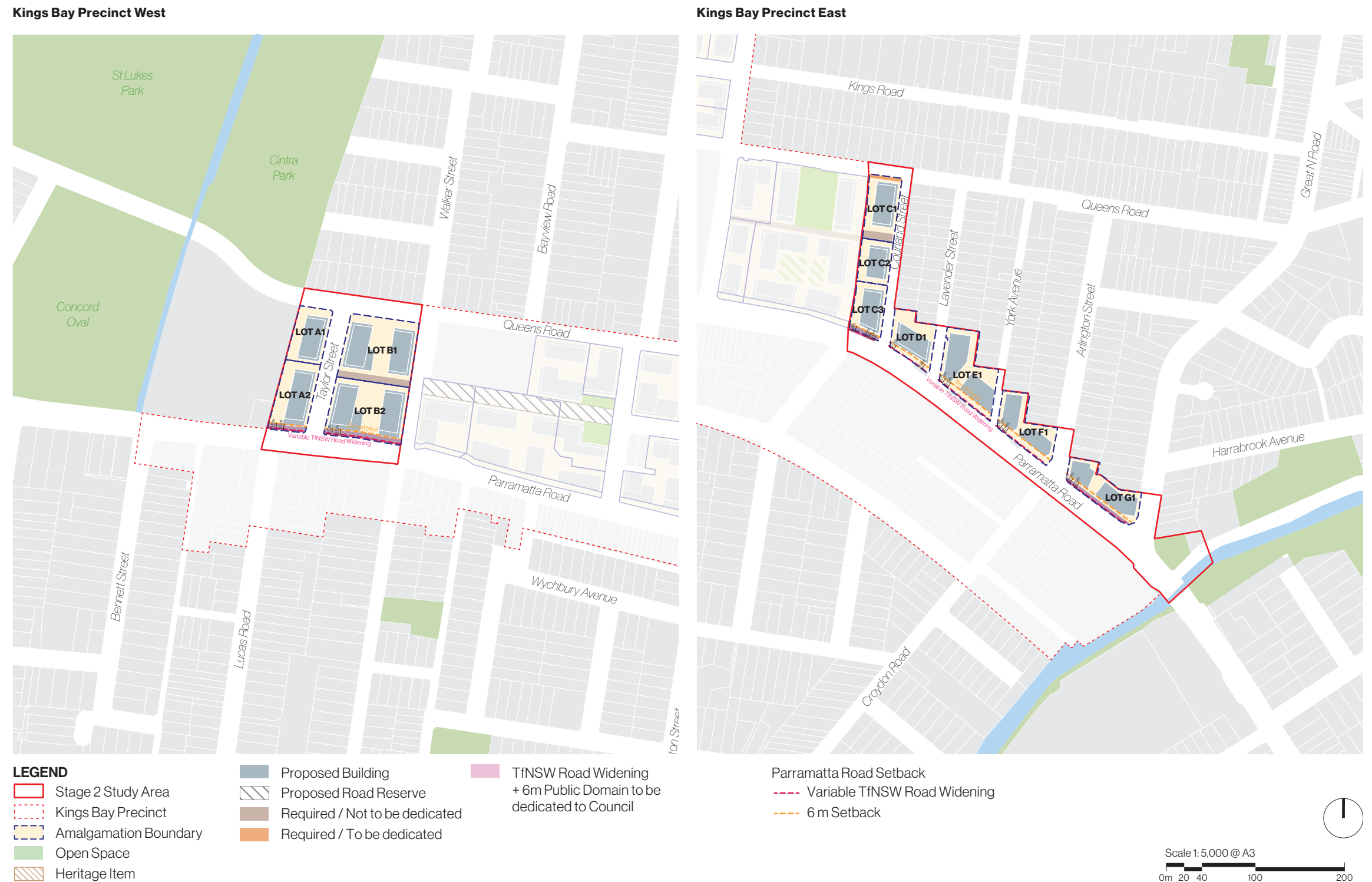
5.4 TRANSPORT FOR NSW ROAD WIDENING

TfNSW requires a modified road reserve to accommodate potential future road widening and public domain enhancement.

Indicative road widenings proposed by TfNSW will be prioritised for public and active transport use, utilised as bus lanes and/or cycle lanes for rapid and suburban routes along Parramatta Road.

A 6m green-edge setback applies along the length of the proposed variable TfNSW road widening, which may extent as far as 8m setback from existing property boundaries along Parramatta Road.

Sections of this 6m green-edge setback may be utilised further public and active transport road widening, minimising potential impact on private property in future.



5.5 LAND DEDICATIONS

Proposed land dedications to Canada Bay Council will contribute to an enhanced public domain and local access to amenity.

Land dedications include through-site links, landscaped setbacks, road reserves, and additional open space, which are to improve interconnectivity and amenity within the Kings Bay Precinct.

Public domain infrastructure that is required to be delivered will remain in private ownership, unless otherwise denoted. The PRCUTS Stage 2 Infrastructure Strategy will describe the infrastructure that is required to be delivered and dedicated to Council, and the infrastructure that is required to be delivered but remain in private ownership.

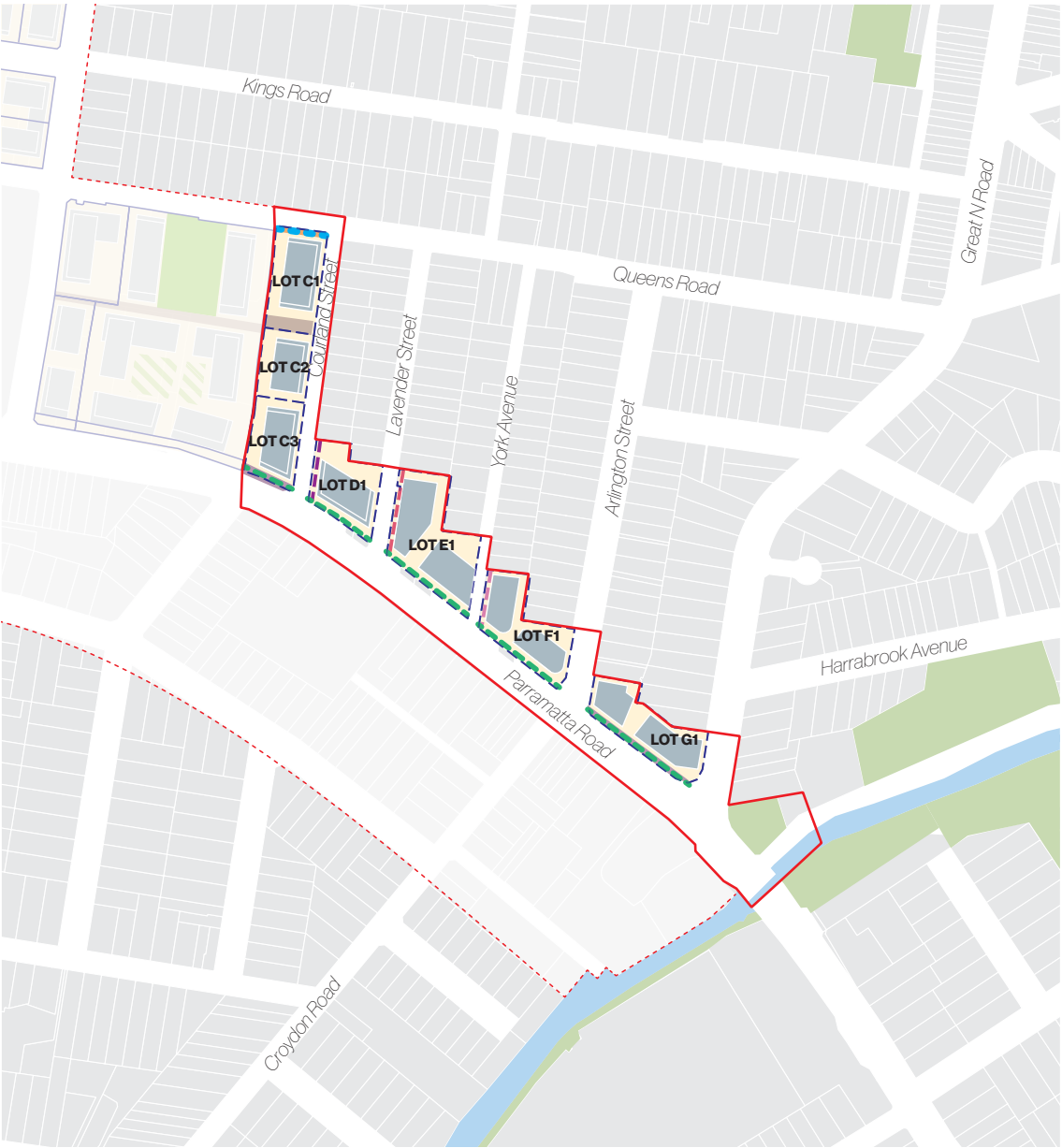
- A 6m through-site link through Lot B1 and Lot B2 will align with the stage one east-west Spencer Street road-extension, permitting only pedestrian foot traffic. 6m landscape setbacks either side contribute to a greener environment.
- A 6m through-site link through Lot C1 lined with landscape setbacks will align with the Stage 1 proposed east-west laneway providing pedestrian access to Courland Street from Harris Street.
- An additional open space on Henley Marine Drive and Great N Road serves as a gateway to the precinct and creates a visual and physical link to Iron Cove Creek.
- A 0.8m, 1m, and 1.4m public domain setback to be dedicated on Lavender Street, York Avenue, and Courland Street respectively.

For more detail on laneways, see section 6.10 Open Space and Links.

Kings Bay Precinct West



Kings Bay Precinct East



LEGEND

- Stage 2 Study Area
- Kings Bay Precinct
- Amalgamation Boundary
- Open Space
- Heritage Item

- Proposed Building
- Proposed Road Reserve
- Required / Not to be dedicated
- Required / To be dedicated

TfNSW Road Widening + 6m Public Domain to be dedicated to Council

Land to be dedicated for public domain enhancement

- 0.8m
- 1m
- 1.4m
- 4m
- 6m

Scale 1:5,000 @ A3
0m 20 40 100 200



5.6 GROUND LEVEL SETBACK

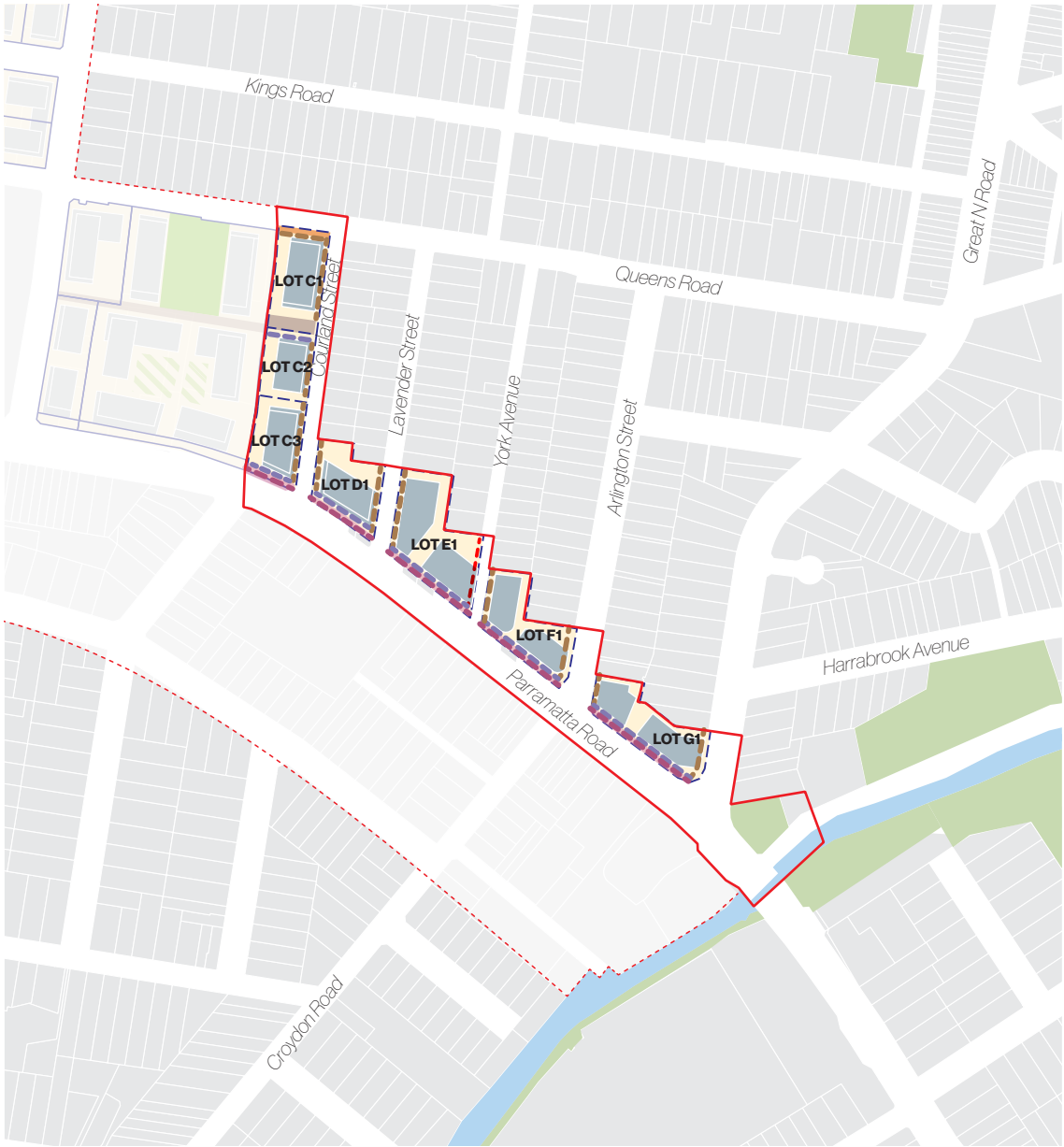
Ground level setbacks are informed by *PRCUTS Planning and Design Guidelines*, and response to context.

- Ground setbacks are generally 3-6m throughout the precinct, with the exception of Parramatta Road which requires a green setback of 6m to allow for public domain improvements from the proposed TfNSW road widening.
- Setbacks do not apply to existing heritage items or buildings, to which proposed built form should align where appropriate to ensure a consistent streetscape.
- For residential buildings, a 3m ground level setback has been applied to internal streets.
 - Increased setbacks have been applied where necessary for ample separation in alignment with NSW Dept. of Planning *Apartment Design Guide*.
 - Setbacks along Queens Road east is 3m allocating a generous space for a landscaped area. There is an additional 4m section that is to be dedicated along Queens Road to align the frontage with the boundary of the adjacent lot boundary.
 - Queens Road west setback is 9m to provide ample separation to northern low-rise residences.
 - Proposed built form in Lot E1 has not setback at Ground Level, to align with the heritage frontage at 3 York Avenue to its immediate north.

Kings Bay Precinct West



Kings Bay Precinct East



LEGEND

- Stage 2 Study Area
- Kings Bay Precinct
- Amalgamation Boundary
- Open Space
- Heritage Item

- Proposed Building
- Proposed Road Reserve
- Required / Not to be dedicated
- Required / To be dedicated

TfNSW Road Widening
+ 6m Public Domain to be
dedicated to Council

- DCP Setbacks
- 0m Setback to Street/Laneway
 - 3m Setback to Street/Laneway
 - 6m Setback to Street/Laneway
 - 9m Setback to Street/Laneway

TfNSW Road Widening
Variable TfNSW
Road Widening

Scale 1: 5,000 @ A3
0m 20 40 100 200

5.8 UPPER LEVEL SETBACKS

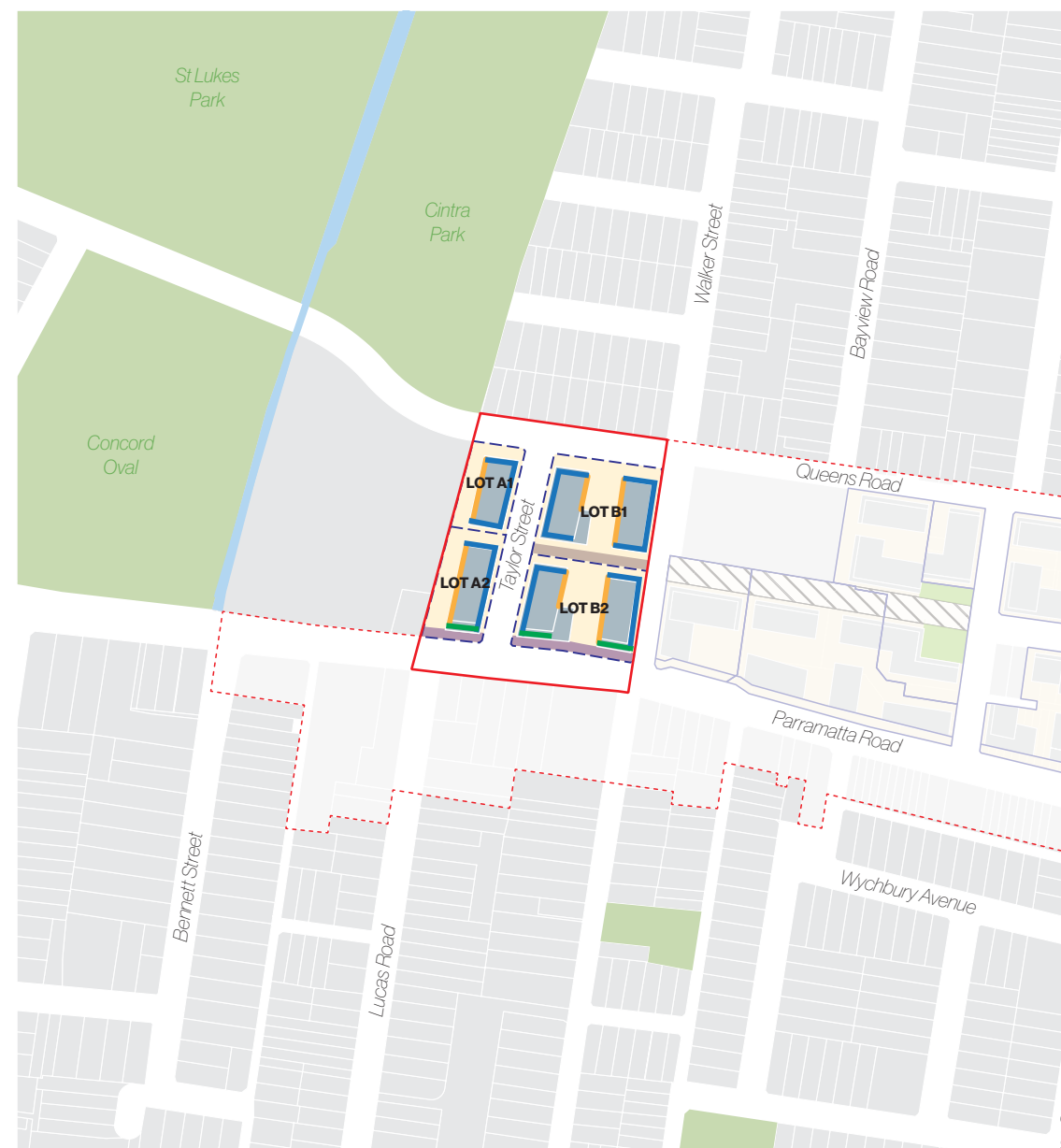
Upper level setbacks are informed by the **PRCUTS Planning and Design Guidelines**, additional setbacks allowing for ample minimise overshadowing and visual impact.

Upper level setbacks are typically 0m, 3m, or 6m from the podium edge, with a wider setback to six storey buildings along Parramatta Road. A 3m upper level setback is appropriate to ensure visual clarity from ground level, creating a consistent two-storey street-wall.

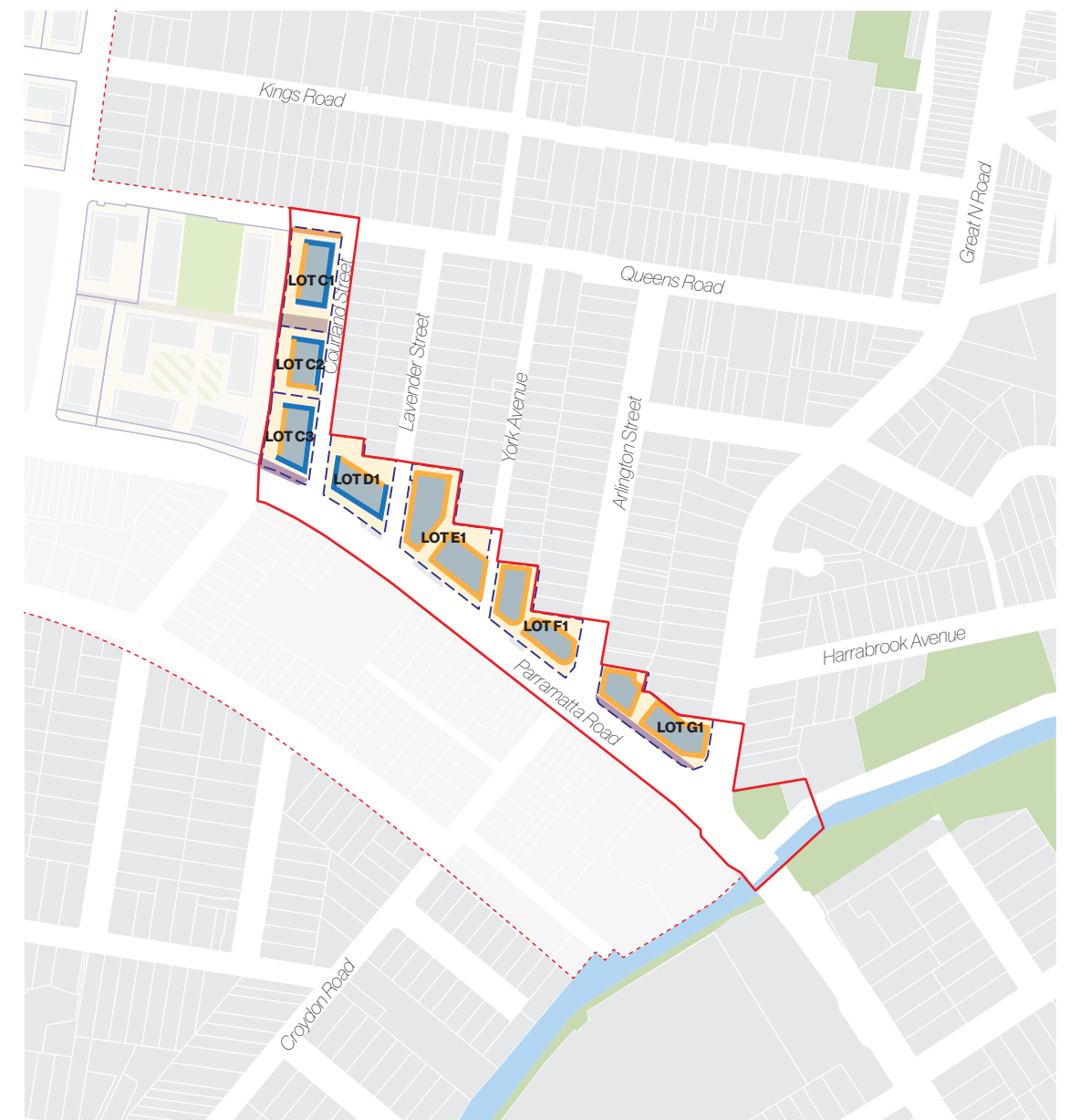
Wider upper level setbacks are required to ensure building separation in alignment with the *Apartment Design Guide* for residential apartment buildings.

- 3m setback from podium toward internal local roads, and to along Parramatta Road.
- 6m upper level setbacks to six storey buildings along Parramatta Road, ensuring visual clarity from ground level, reducing noise and visual impact from Parramatta Road.

Kings Bay Precinct West



Kings Bay Precinct East



LEGEND

- Stage 2 Study Area
- Kings Bay Precinct
- Amalgamation Boundary
- Open Space
- Heritage Item

- Proposed Building
- Proposed Road Reserve
- Required / Not to be dedicated
- Required / To be dedicated

- TfNSW Road Widening + 6m Public Domain to be dedicated to Council

- Upper Level Setback
- 0m Upper Level Setback
- 3m Upper Level Setback
- 6m Upper Level Setback

*From edge of podium

Scale 1:5,000 @ A3
0m 20 40 100 200

5.9 STREET WALL

A street wall height of two metres has been applied throughout the Kings Bay Precinct, informed by intended street character, building typology, and response to context.

A consistent street wall height of two storeys ensures a street-scape and public domain which responds to the pedestrian scale and integrates with adjacent existing single storey dwellings. Proposed built form above two storeys is setback further from the street.

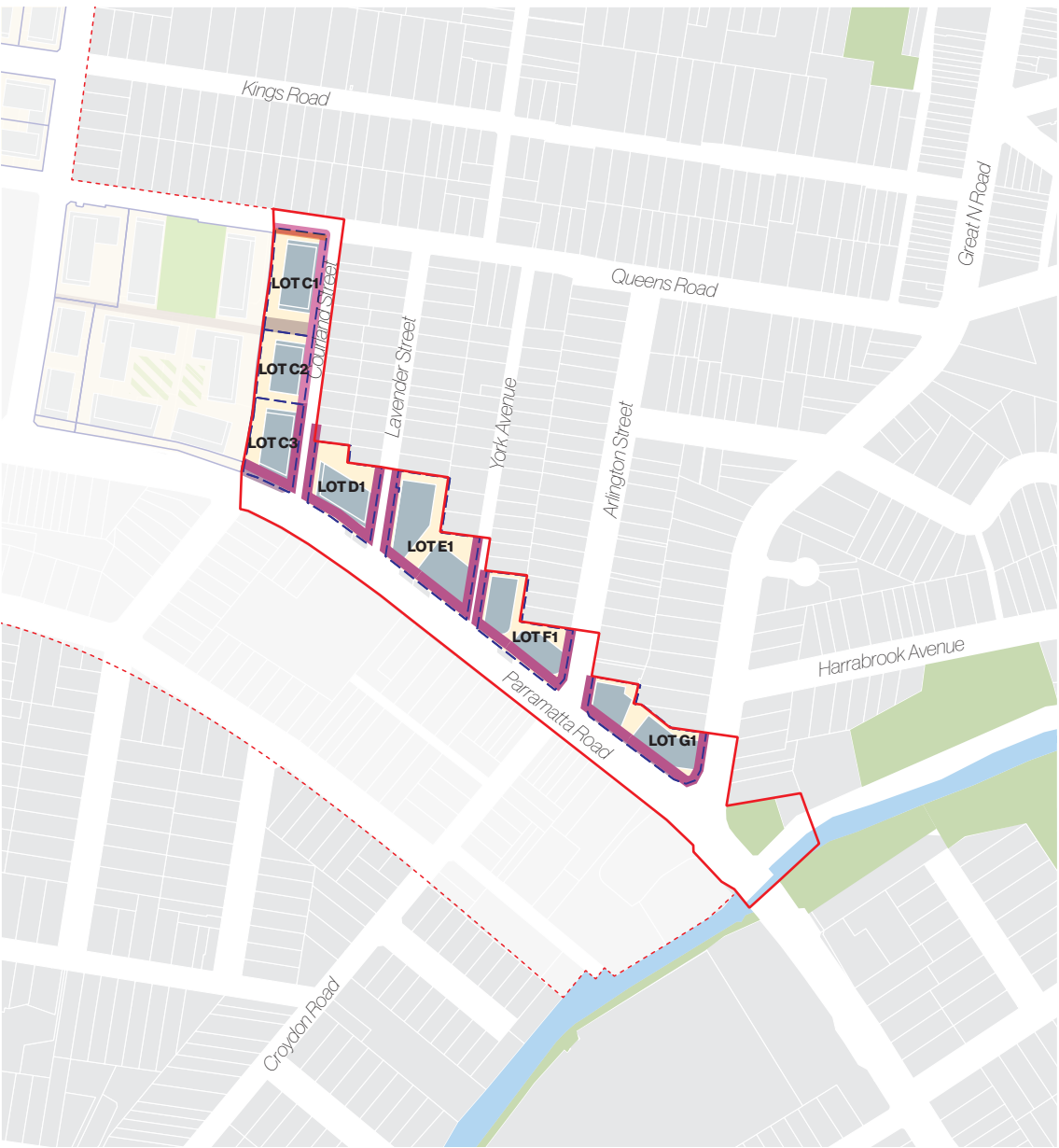
Kings Bay Precinct Stage 2 is largely residential in character along local roads, requiring a street-scape which reflects its low-rise residential context.

- Four storey street wall along internal local roads and Queens Road with the exception of Lot C1 and Lot C2 on Courland Street which features two storey street wall to respond to existing low-rise residential context and ensure smooth transition.
- Four storey street wall along Parramatta Road, with an active ground level frontage.

Kings Bay Precinct West



Kings Bay Precinct East



LEGEND

- Stage 2 Study Area
- Kings Bay Precinct
- Amalgamation Boundary
- Open Space
- Heritage Item

- Proposed Building
- Proposed Road Reserve
- Required / Not to be dedicated
- Required / To be dedicated

- TfNSW Road Widening + 6m Public Domain to be dedicated to Council
- 2-storey Street Wall
- 4-storey Street Wall

Scale 1: 5,000 @ A3

5.10 OPEN SPACE AND LINKS

Additional pedestrian through-links, cycle paths, and public open space will contribute to the public domain character and amenity of the Kings Bay Precinct.

Proposed through-site links are outlined in *PRCUTS Planning and Design Guidelines* in addition to desired through-site links which have been integrated into the stage two proposal, to ensure a permeable and pedestrian-oriented public domain.

Cycle paths as proposed in *Parramatta Road Public Domain Plan Stage 2* and Canada Bay Council's *Draft Bike Plan* have been integrated into the Stage 2 proposal.

- On-road cycleway on either side of Queens Road and shared cycleway along length of Parramatta Road extending stage one proposed cycleway.
- Through-site link Taylor Street to Walker Street through Lots B1 and B2, extending the proposed Spencer Street road reserve datum.
- Through-site link Courland Street to Harris Road, extending the proposed stage one through-site link.

Kings Bay Precinct West



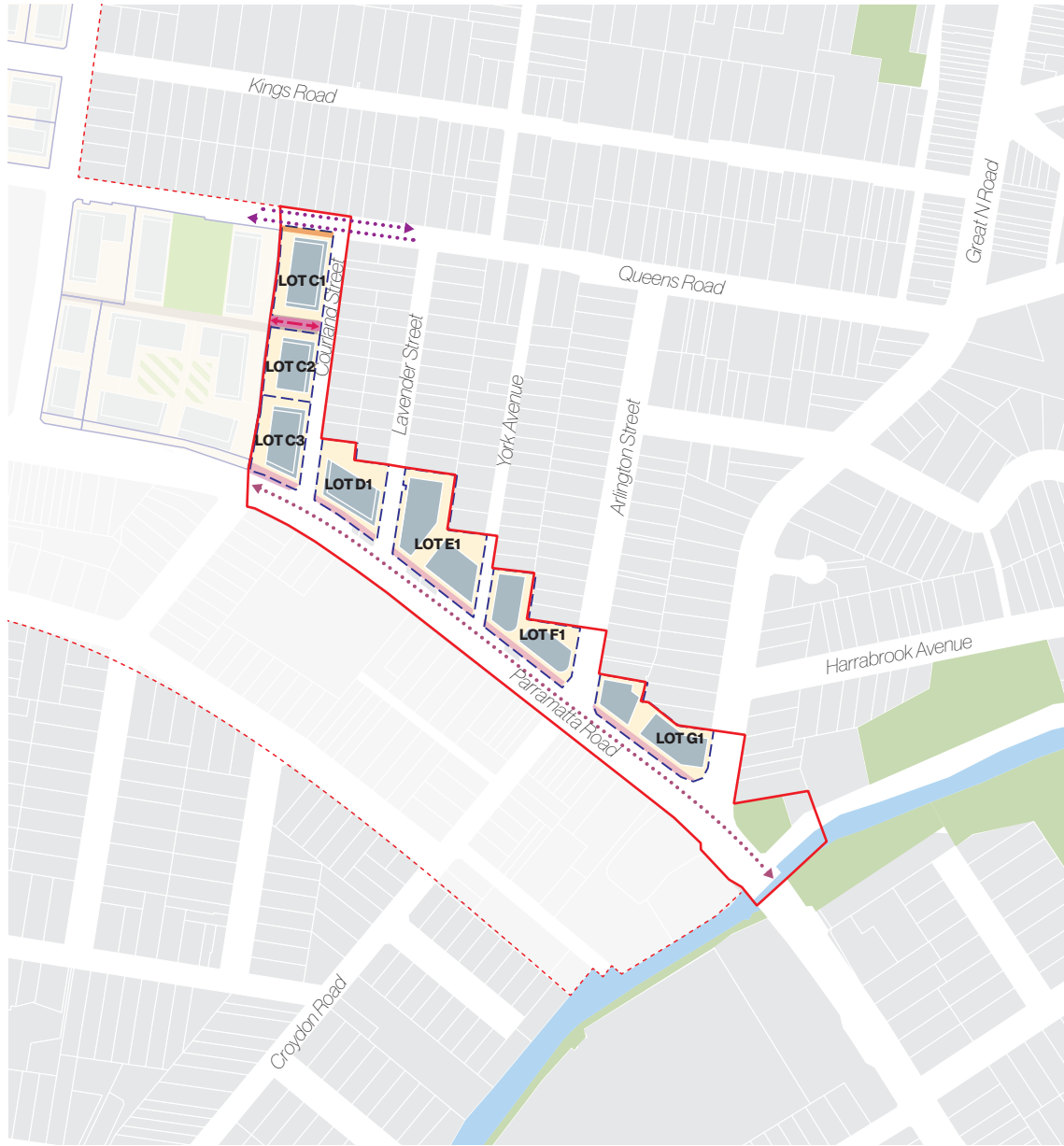
LEGEND

- Stage 2 Study Area
- Kings Bay Precinct
- Amalgamation Boundary
- Open Space
- Heritage Item

- Proposed Building
- Proposed Road Reserve
- Required / Not to be dedicated
- Required / To be dedicated

TfNSW Road Widening + 6m Public Domain to be dedicated to Council

Kings Bay Precinct East



- New road
- Through-site Link
- Proposed Through-site Link
- On-road Cycleway
- Shared Cycleway

Pedestrian Links with Floor Space Reallocation

- 6m laneway

Scale 1: 5,000 @ A3

0m 20 40 100 200

5.11 ACTIVE FRONTAGES

The location and typology of active frontages are informed by their intended local character and proximity to pedestrian amenity.

Frontage typologies have been adapted from Jan Gehl's *Cities for People* to respond to the conditions of the Parramatta Road corridor.

More vibrant facades are appropriate for areas of high pedestrian activity and long dwelling times.

Vibrant Facades

- Narrow units with a minimum of 15 frontages per 100m facade length.
- Cater for wide variety of retail, commercial, and community uses.
- High degree of visual variety, detailed facade expression, and clarity with permeable frontages.
- Vehicular access and servicing zones not permitted.

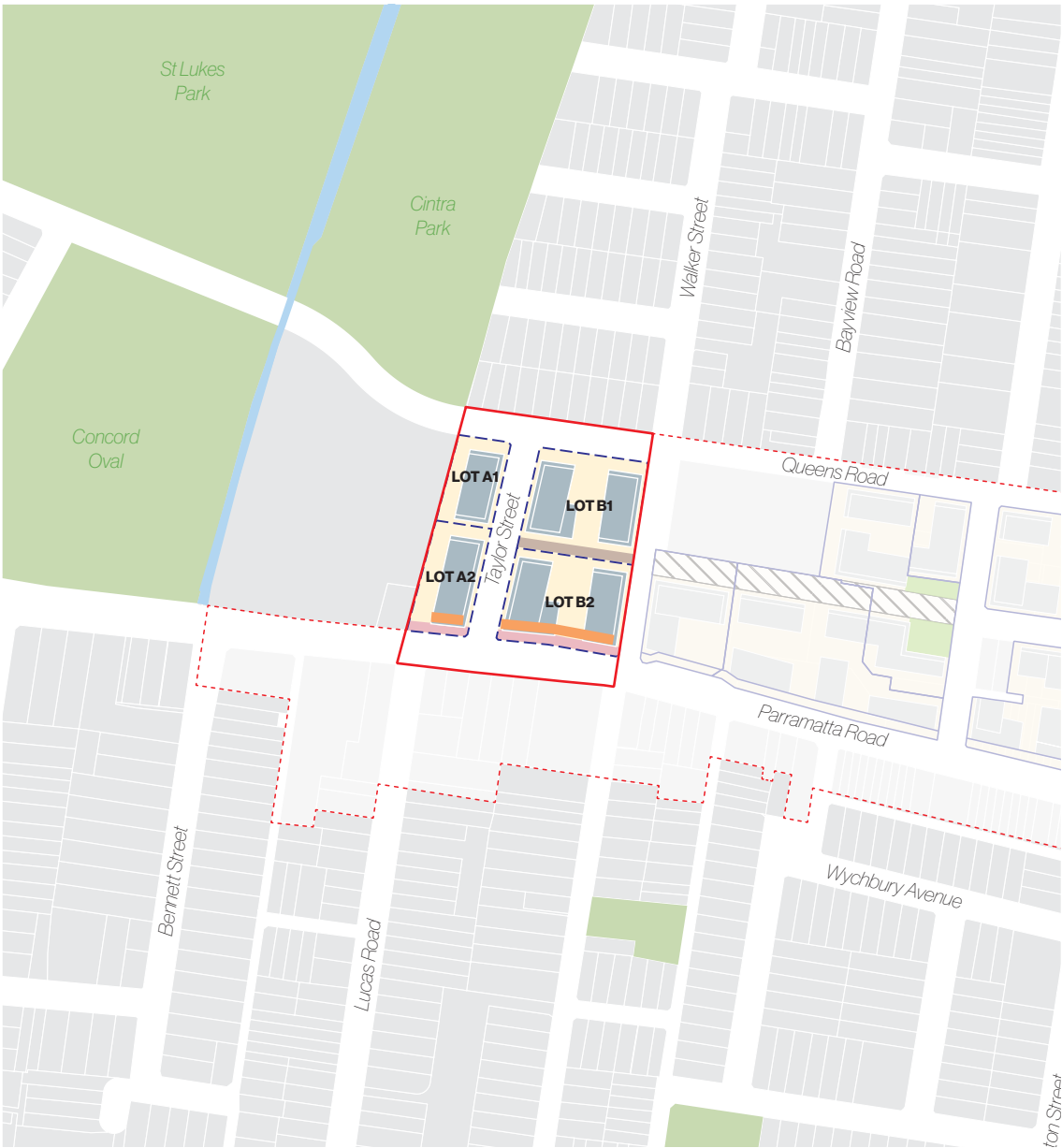
Friendly Facades

- Relatively narrow units with a minimum of 10 frontages per 100m facade length.
- Cater for variety of retail, commercial, community, and residential uses.
- Moderate degree of visual variety and facade expression.
- Vehicular access and servicing zones permitted where required.

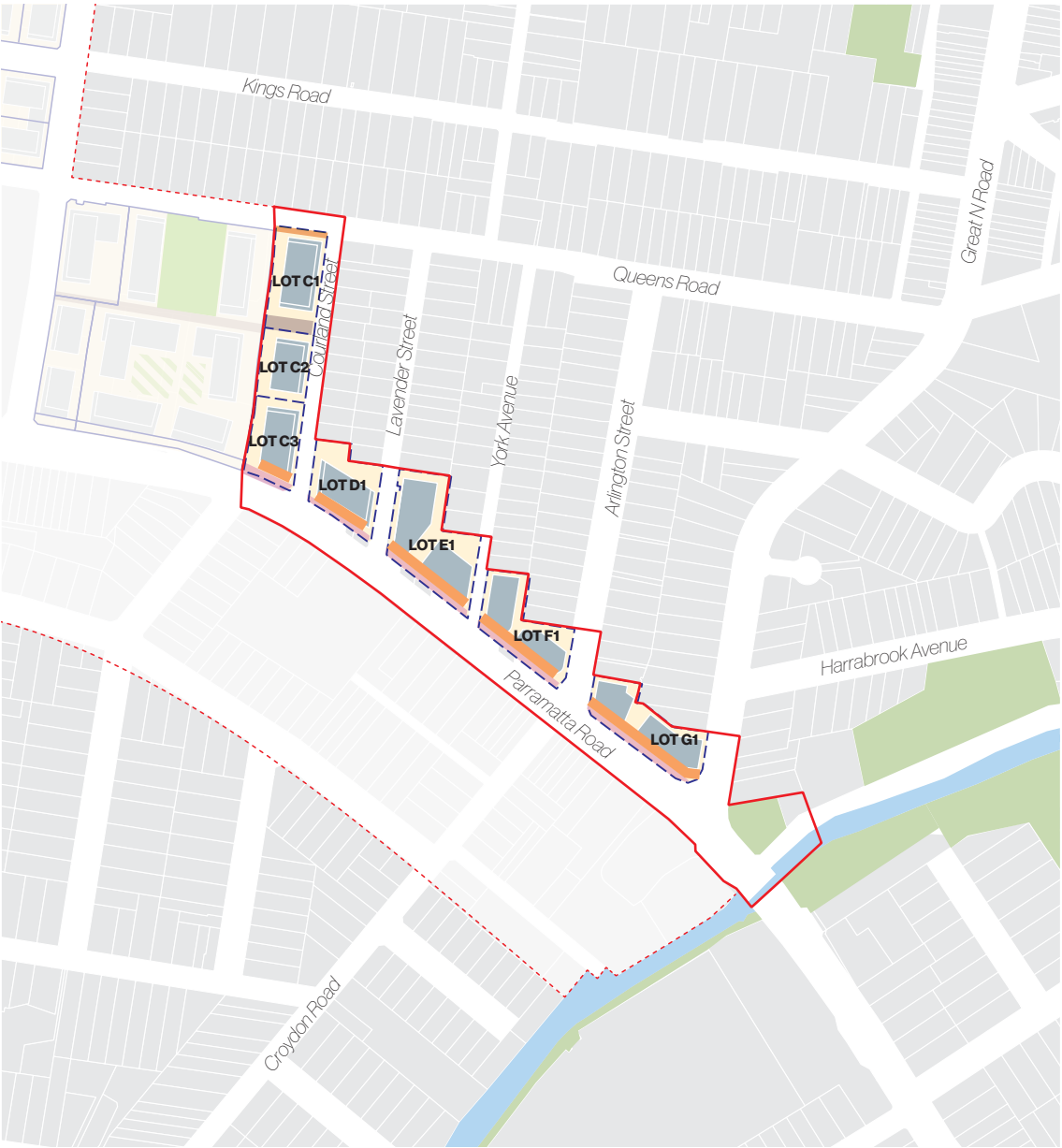
Mixed Facades

- Minimum of 6 frontages per 100m facade length.
- Cater for primarily commercial and residential use.
- Blank facades of over 10% of facade of 10m² require facade expression or treatment.
- Vehicular access and servicing zones permitted, other than along Parramatta Road where underground parking will be located on side streets.

Kings Bay Precinct West



Kings Bay Precinct East



LEGEND

- Stage 2 Study Area
- Kings Bay Precinct
- Amalgamation Boundary
- Open Space
- Heritage Item

- Proposed Building
- Proposed Road Reserve
- Required / Not to be dedicated
- Required / To be dedicated

TfNSW Road Widening
+ 6m Public Domain to be
dedicated to Council

- Vibrant Facades
- Friendly Facades
- Mixed Facades

Scale 1: 5,000 @ A3
0m 20 40 100 200

5.12 PROPOSED REZONING

Amendments to PRCUTS recommended land zoning are proposed to accommodate strategic uses and encourage the intended land use pattern.

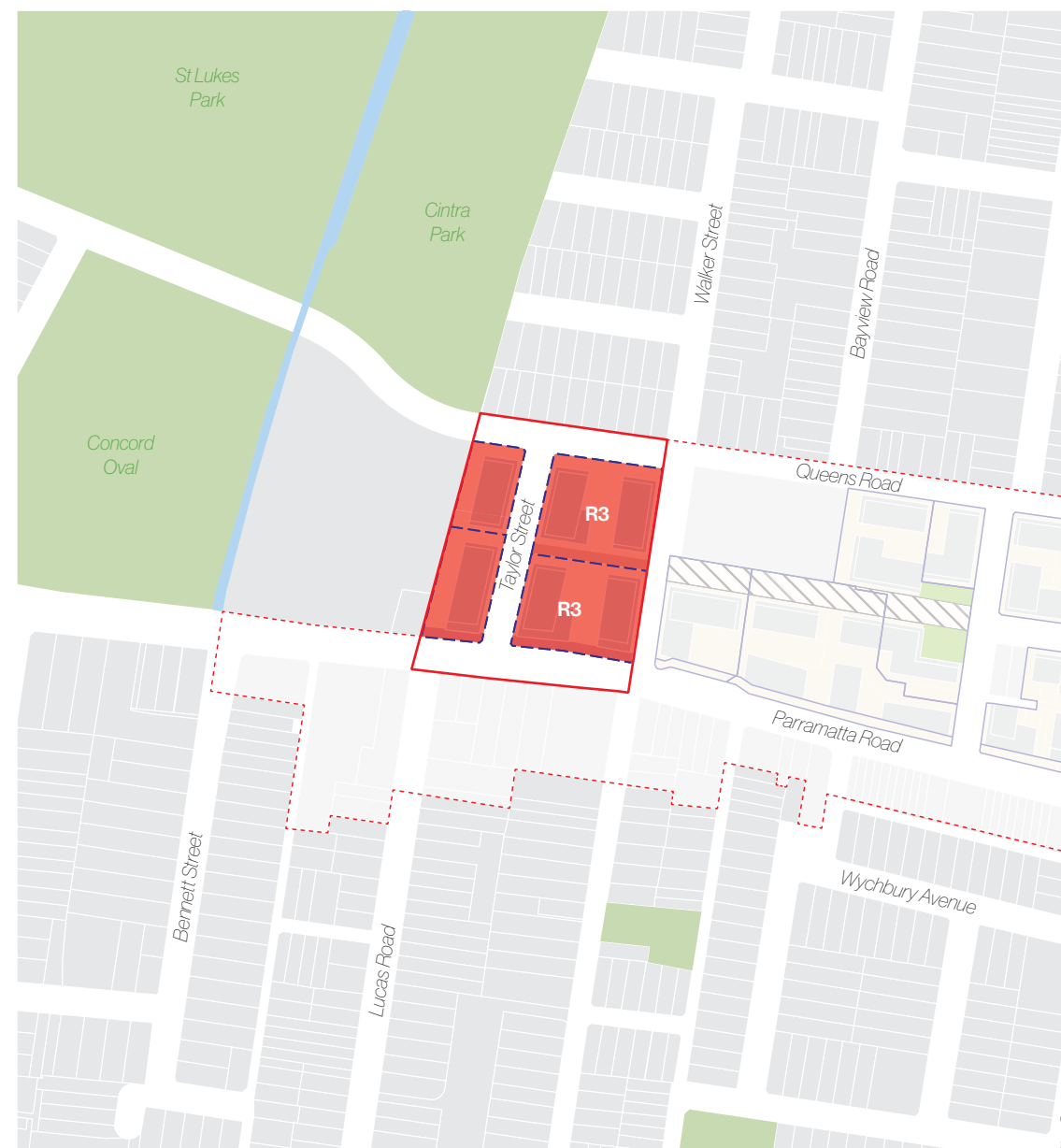
As of April 2023 amendments to the SEPP (Land Use Zones) 2023 land zoning within Canada Bay Council has been consolidated, converting the B4 Mixed Use zone into the MU1 Mixed Use zone.

Whilst PRCUTS recommended E3 to all land east of Courland Street, the Masterplan is proposing the precinct to be all R3, but with additional permitted uses to enable commercial and light industrial uses fronting Parramatta Road. There will also be Active Frontages to the Parramatta Road frontage.

Within the Kings Bay Precinct amalgamated lots are proposed for rezoning to R3 Medium Density Residential, with stand-alone open space zoned RE1 Public Recreation.

- Proposed R3 Medium Density Residential along internal local roads.
- Proposed open space on Henley Marine Drive zoned RE1 Public Recreation.

Kings Bay Precinct West



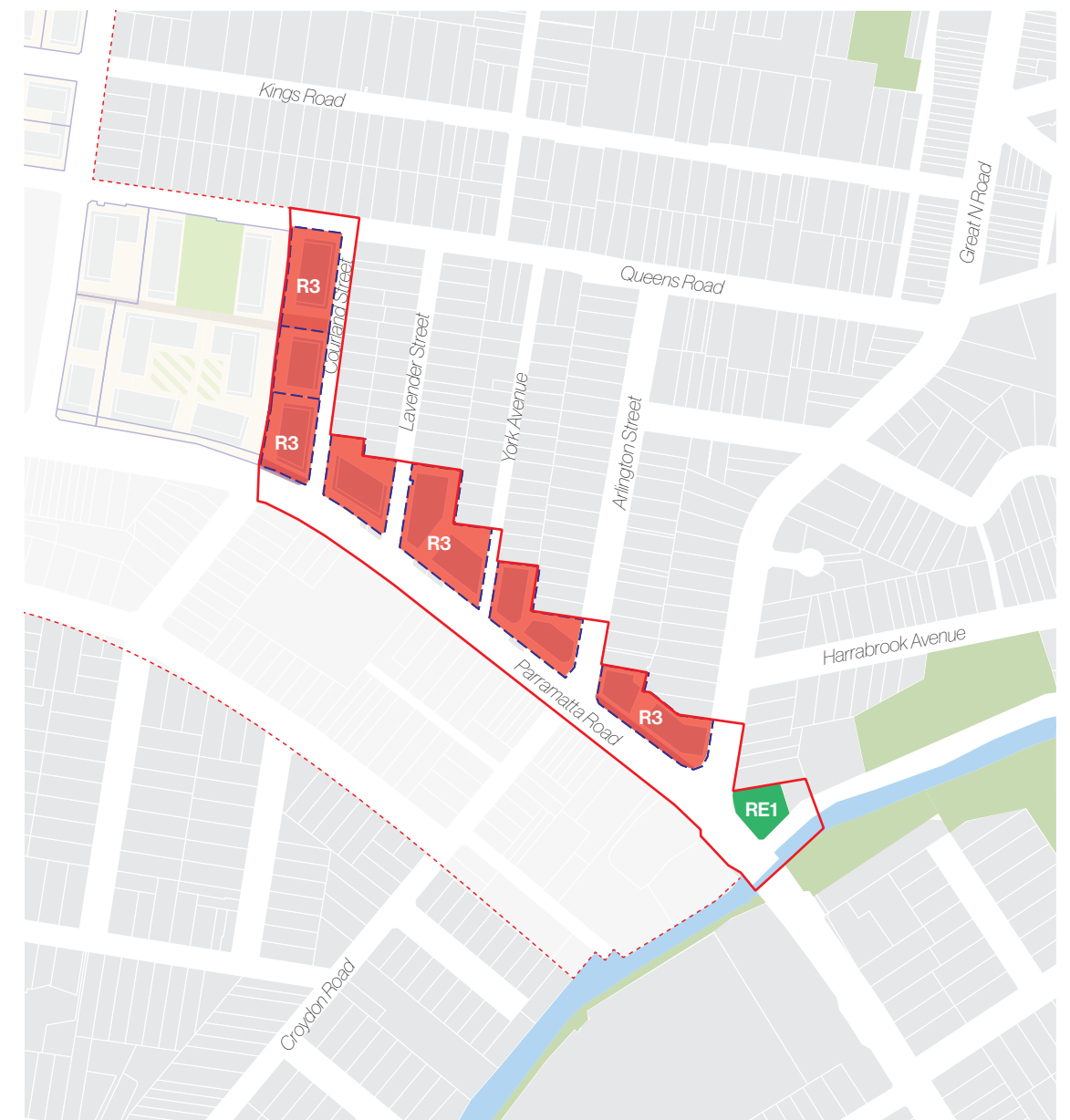
LEGEND

- Stage 2 Study Area
- Kings Bay Precinct
- Amalgamation Boundary
- Open Space
- Heritage Item

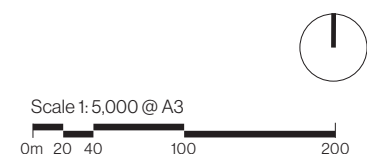
- Proposed Building
- Proposed Road Reserve
- Required / Not to be dedicated

TfNSW Road Widening
+ 6m Public Domain to be
dedicated to Council

Kings Bay Precinct East



- R3 Medium Density Residential
- RE1 Public Recreation



5.13 SOLAR ANALYSIS

Kings Bay West Facades North-East

The master plan contains several lots that are subject to compliance with the Apartment Design Guide (ADG).

Per objective 4A-1, the proposal seeks to optimise the number of apartments receiving sunlight to

habitable rooms, primary windows and private open space, that can be assessed using the following design criteria;

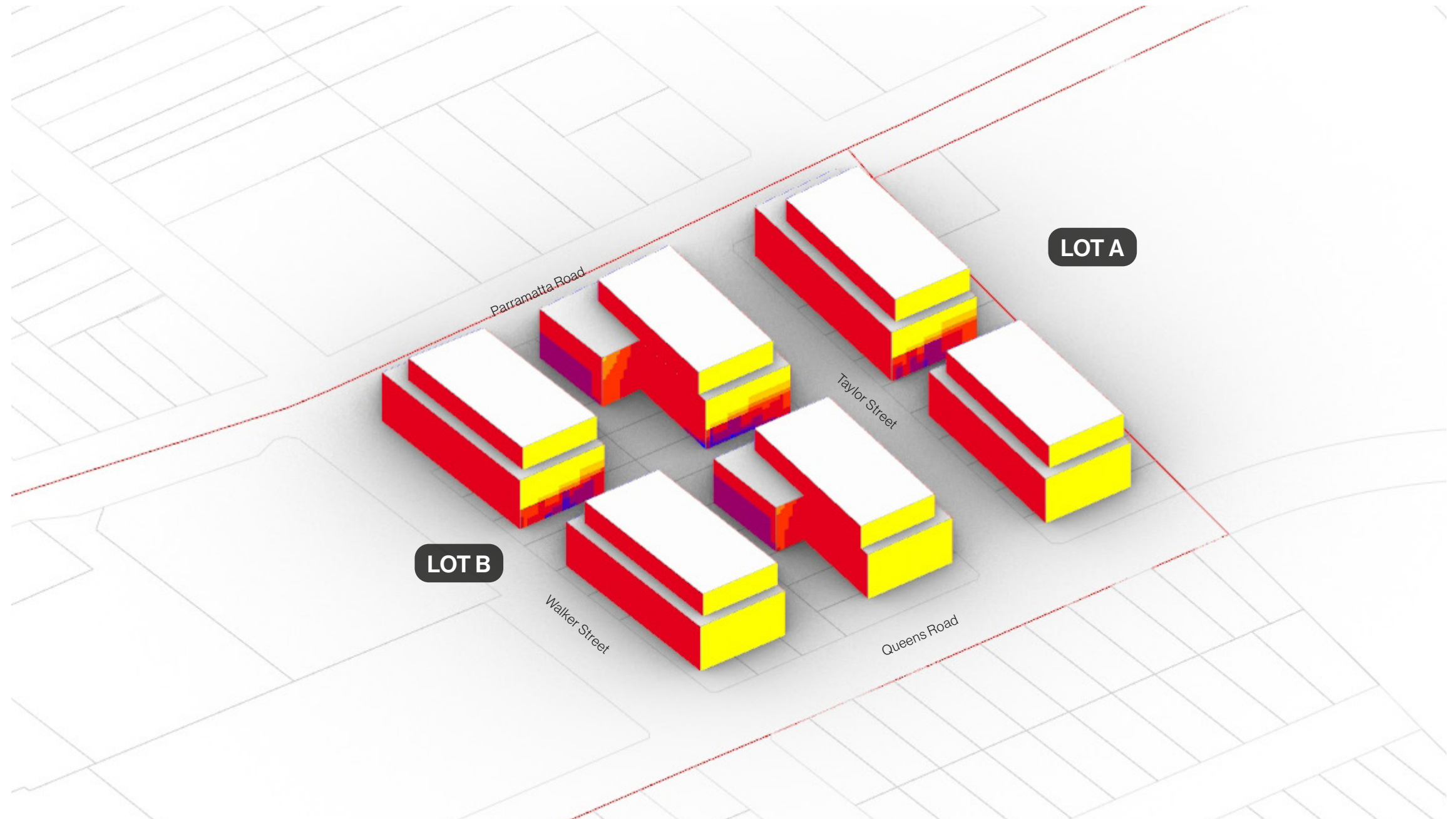
1. Living rooms and private open spaces of at least 70% of apartments in a building receive a minimum of 2 hours direct sunlight between 9 am and 3 pm at mid winter in the Sydney Metropolitan Area and in the Newcastle and Wollongong local government areas
2. In all other areas, living rooms and private open spaces of at least 70% of apartments in a building receive a minimum of 3 hours direct sunlight between 9 am and 3 pm at mid winter
3. A maximum of 15% of apartments in a building receive no direct sunlight between 9 am and 3 pm at mid winter

Our high level analysis resulted in the following percentage of apartments receive the minimum required two hours of sunlight between 9am -3pm on 21st June:

Lot A	99.8%
Lot B	92.4%

Whole of Precinct Apartments: 94.8%

We recognise that this is a high level assessment, based on building envelopes only and that with further detailed design and planning, compliance is likely to be achieved.



LEGEND

Study Area

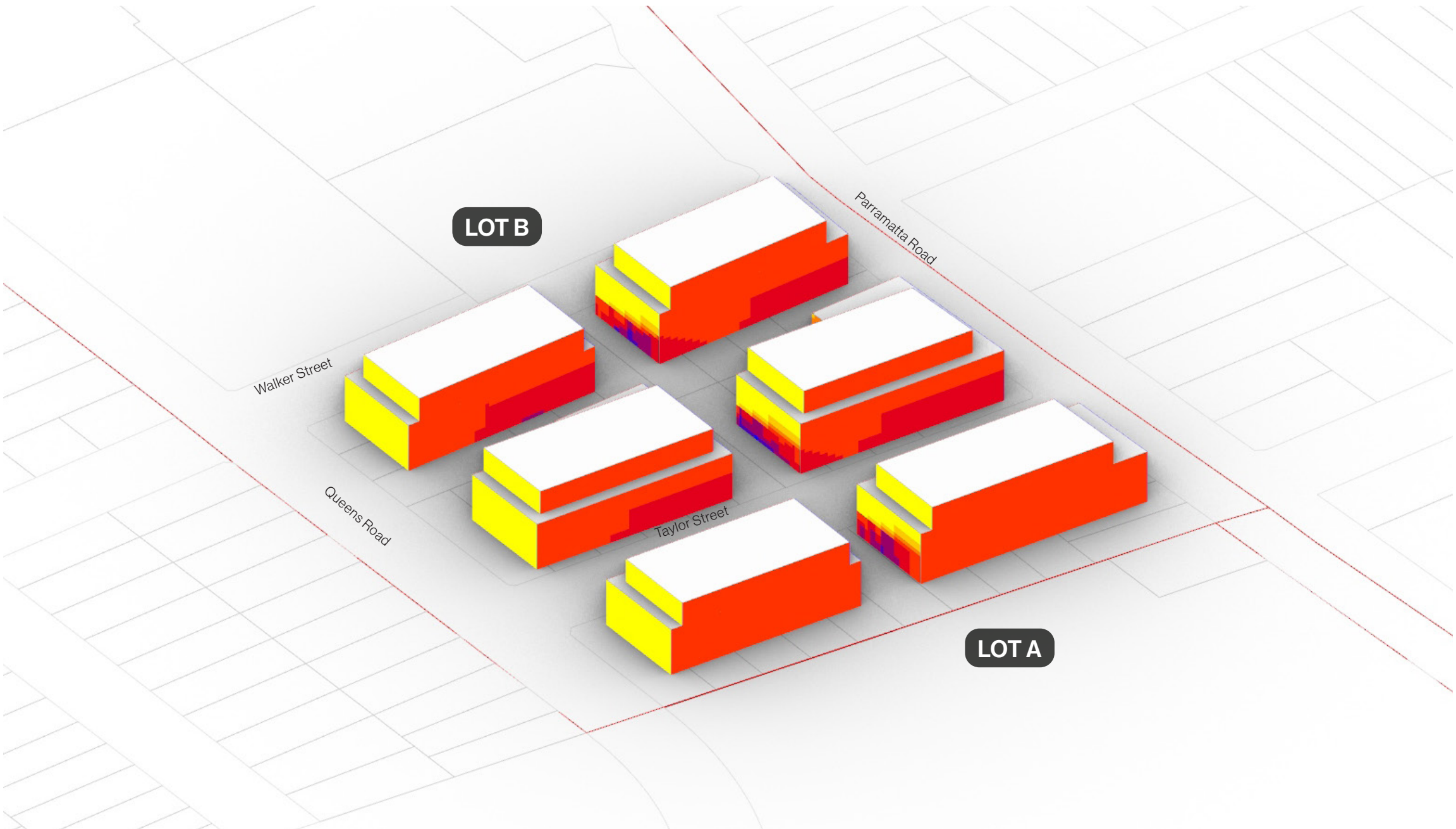
Winter Solstice Solar Access

0 - 1hr
1 - 2hr
2-3hr
3-4hr

4-5hr
5-6hr
6-7hr
>7hr

Built form not subject to ADG solar compliance

Kings Bay West
Facades North-West



LEGEND

Study Area

Winter Solstice Solar Access

- | | |
|---------|-------|
| 0 - 1hr | 4-5hr |
| 1 - 2hr | 5-6hr |
| 2-3hr | 6-7hr |
| 3-4hr | >7hr |

Built form not subject to
ADG solar compliance

**Kings Bay East
Facades North-East**

The master plan contains several lots that are subject to compliance with the Apartment Design Guide (ADG).

Per objective 4A-1, the proposal seeks to optimise the number of apartments receiving sunlight to

habitable rooms, primary windows and private open space, that can be assessed using the following design criteria;

- 1. Living rooms and private open spaces of at least 70% of apartments in a building receive a minimum of 2 hours direct sunlight between 9 am and 3 pm at mid winter in the Sydney Metropolitan Area and in the Newcastle and Wollongong local government areas
- 2. In all other areas, living rooms and private open spaces of at least 70% of apartments in a building receive a minimum of 3 hours direct sunlight between 9 am and 3 pm at mid winter
- 3. A maximum of 15% of apartments in a building receive no direct sunlight between 9 am and 3 pm at mid winter

Our high level analysis resulted in the following percentage of apartments receive the minimum required two hours of sunlight between 9am -3pm on 21st June:

Lot C	99.6%
Lot D	75.9%
Lot E	86.2%
Lot F	82.7%
Lot G	73.8%

Whole of Precinct Apartments: 86.6%

We recognise that this a high level assessment, based on building envelopes only and that with further detailed design and planning, compliance is likely to be achieved.



LEGEND

Study Area

Winter Solstice Solar Access

 0 - 1hr	 4-5hr
 1- 2hr	 5-6hr
 2-3hr	 6-7hr
 3-4hr	 >7hr

Built form not subject to ADG solar compliance

**Kings Bay East
Facades North-West**



5.14 OVERSHADOWING

The proposed built form has been developed to minimise solar impact on its immediate context and to neighbouring dwellings.

Diagrams illustrate the maximum potential overshadowing of proposed built form taken on June 22nd winter solstice at 9am, 12pm, and 3pm.

Proposed built form within Kings Bay Precinct East have a minimal impact on surrounding context, overshadowing lots to the precinct's south at 9am, and lots to the precinct's east within the stage one study area at 3pm.

Internal overshadowing is minimal, with north-facing facades receiving solar access at 12pm winter solstice.



The proposed built form has been developed to minimise solar impact on its immediate context and to neighbouring dwellings.

Diagrams illustrate the maximum potential overshadowing of proposed built form taken on June 22nd winter solstice at 9am, 12pm, and 3pm.

Proposed built form within Kings Bay Precinct West have a minimal impact on surrounding context, with nil impact on lots to the precinct's south.

Overshadowing is most prominent on Courland Street during afternoon hours on residential lots to the precinct's east.



22 June - 9am



22 June - 12pm



22 June - 3pm

[Redacted]





6.0 STREET SECTIONS

06

6.1 KINGS BAY PRECINCT WEST

Parramatta Road West

The character of Parramatta Road has been designed to integrate the street’s existing role as a major city thoroughfare with the proposed retail and commercial hub of Kings Bay.

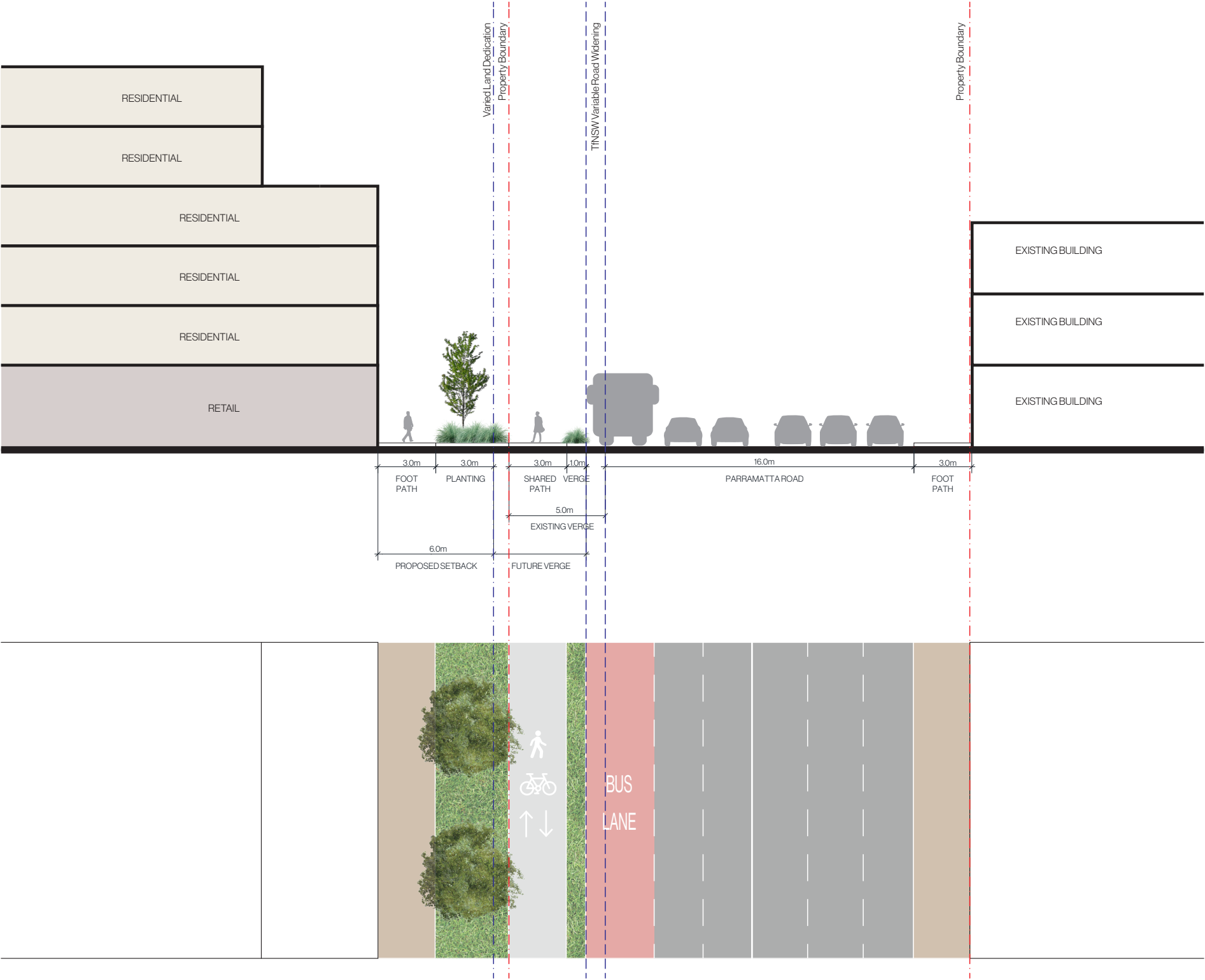
Proposed shared path and pedestrian footpaths will enhance the transport infrastructure of the street. Significant portions of landscape planting areas will be accommodated within a wide setback of 6m and a variable road widening (TfNSW).

This wide setback will not only provide a sense of relief for residents and pedestrians but also facilitate a smooth transition from the bustling environment of Parramatta Road.

The proposed built form along Parramatta Road West will feature a 4-storey street wall. The upper two levels are setback by 6 meters to ensure a human-scale experience for pedestrians and reduce noise impact from Parramatta Road for the residents.

The proposed buildings will incorporate ground floor retail spaces and commercial offices, effectively supporting business activities along the corridor and serving as a transport hub.

In areas prone to flooding, it is mandated that buildings have their ground floors elevated to 400mm above the existing ground level.



*Note: Refer to page 40 for Variable TfNSW Road Widening

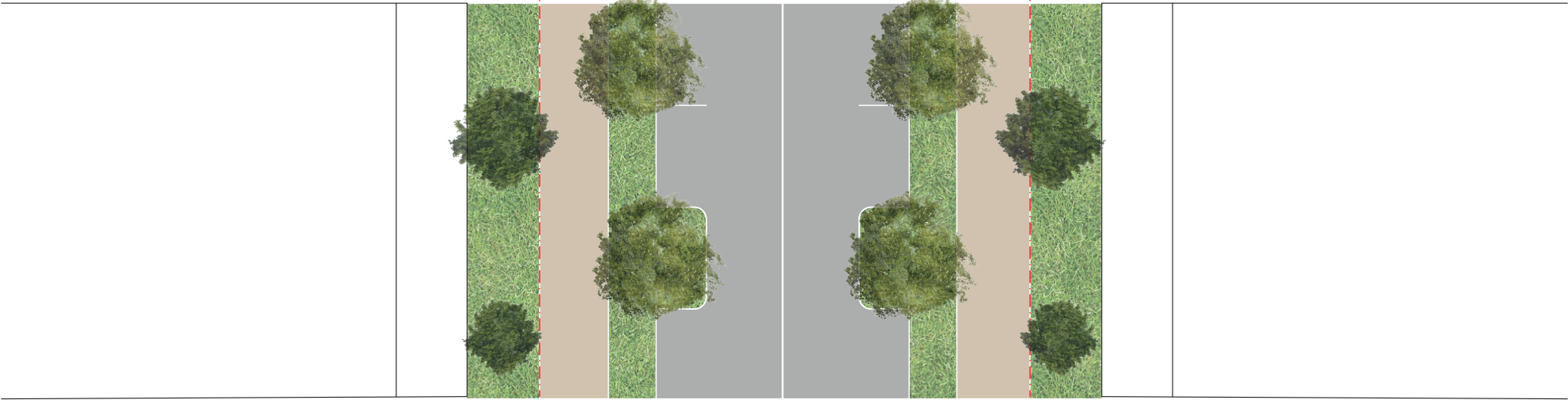
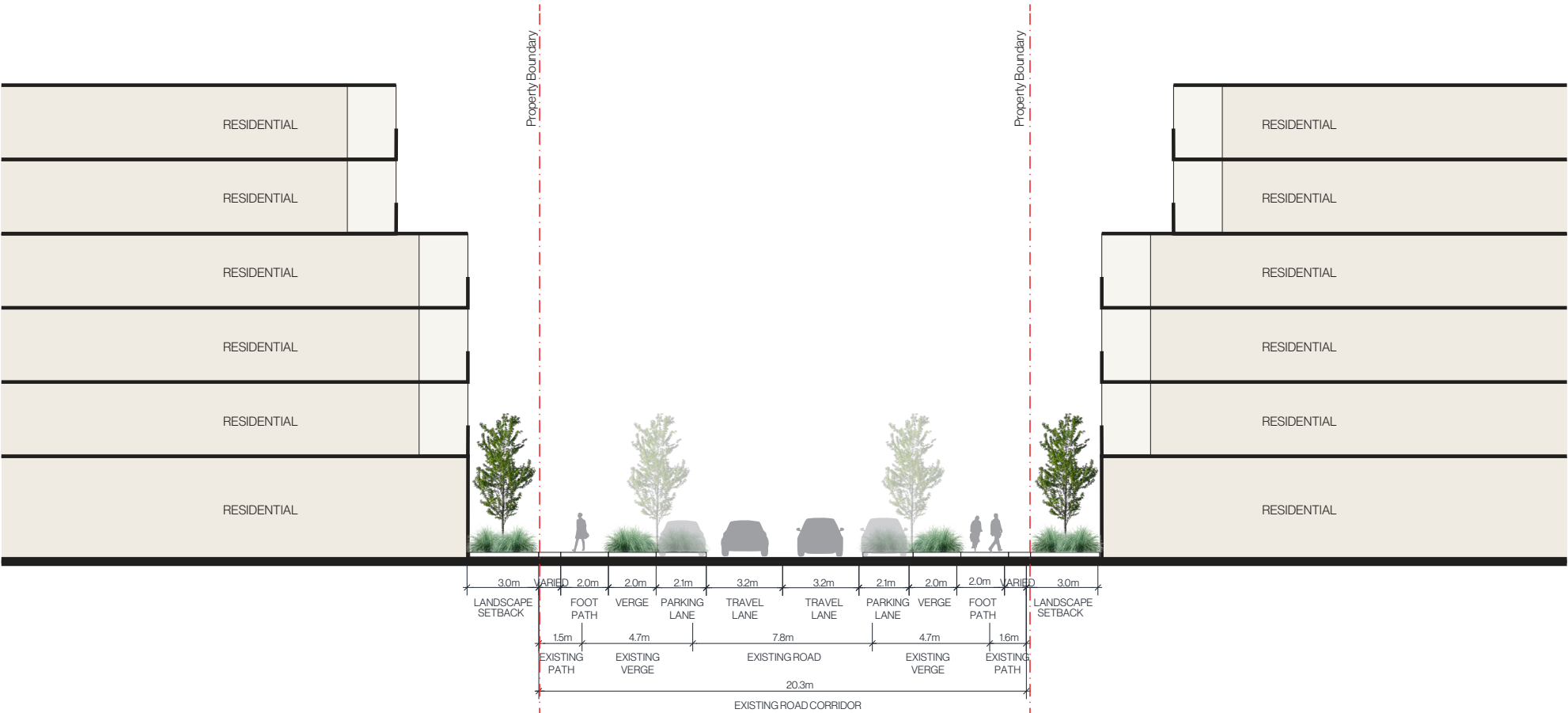


Taylor Street

The urban design of Taylor Street will transition the activity of the proposed Kings Bay commercial hub to the lower pace, density and heights of the surrounding residential streets.

A 4-storey street wall, with 2 storeys above will transition the character of the precinct's built form to a more comfortable, domestic scale.

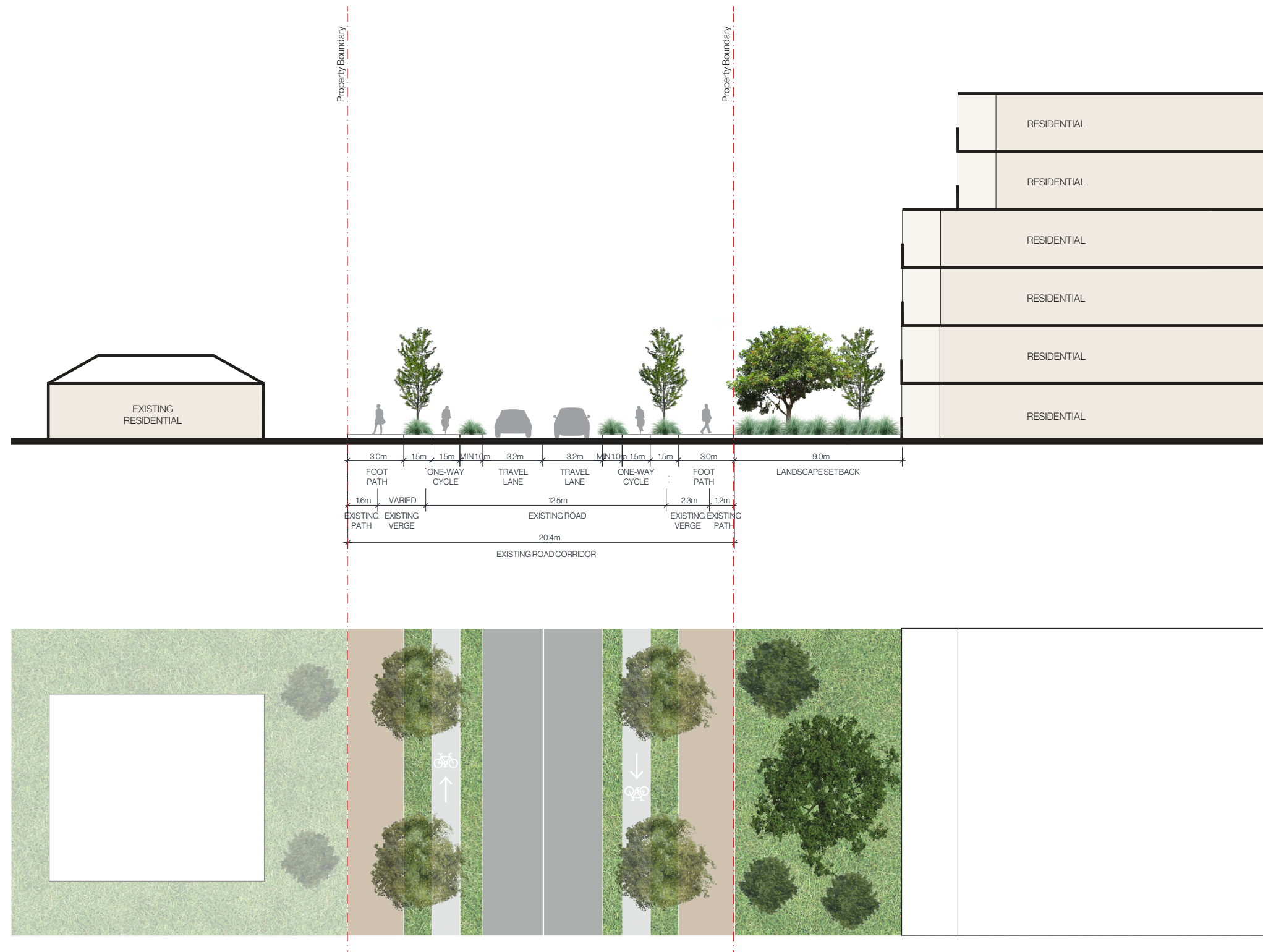
A 3m landscape setback will enhance passive surveillance and connectivity from the townhouses to the street by generating a comfortable thoroughfare for pedestrian travel.



The proposed character of Queens Road aims to transition in density to the north of the precinct with a generous landscape setback that prioritises pedestrian movement and street-level engagement.

A one way cycleway separated by planting on both sides of the street will improve connectivity of the precinct and enhance safety and visibility between pedestrians, cyclists and motorists.

Landscape design along Queens Road will prioritise the integration of trees and greenery, increasing canopy cover along the street. This will not only enhance the visual appeal but also provide shade and contribute to a more sustainable and comfortable environment for pedestrians.



Spencer Street Link

A new pedestrianised link will connect the proposed Spencer Street Plaza and through site link to terminate on Taylor Street.

The link will feature a flush, paved ground surface to delineate the link as a pedestrian zone.

This thoroughfare will link the public realm along Spencer Street Plaza to the wider green grid and sports facilities to the west of the precinct.

A proposed 6m landscape setback on both sides of the through site link zone will provide space for ecological links, enhance passive surveillance and improve connectivity by generating a comfortable pedestrian environment through the development.

A 4-storey street wall sits with the overall scale of the surrounding through-site link facilitating a gradual transition in height from the precinct's central area to the single-storey, detached residential properties that sit to the north.

To ensure a human-scale experience for pedestrians, the upper level of the building is setback by 3 meters, which creates space for an additional two storeys.

Buildings along Spencer Street Link will accommodate predominantly residential uses.



6.2 KINGS BAY PRECINCT EAST

Courland Street

Courland Street will be characterised by plentiful planted beds lining comfortable paths of travel for motorists, cyclists and for pedestrian movement.

The existing 1.4m footpath within the property boundary on the eastern side of Courland Street will be dedicated to Council improving the streetscape corridor. A proposed 3m setback from the property boundary on both sides of the road will permit ample landscaped areas.

A 4-storey street wall has been designed to sympathetically interface with the scale of the street and to begin the transition of the height of the precinct's centre to the single storey, detached residential properties at its boundary. Buildings along Courland Street will accommodate predominantly residential uses, with ground floor retail space proposed along Parramatta Road.



Queens Road East

The proposed character of Queens Road aims to significantly enhance east west accessibility along the street by prioritising pedestrian movement and street-level engagement.

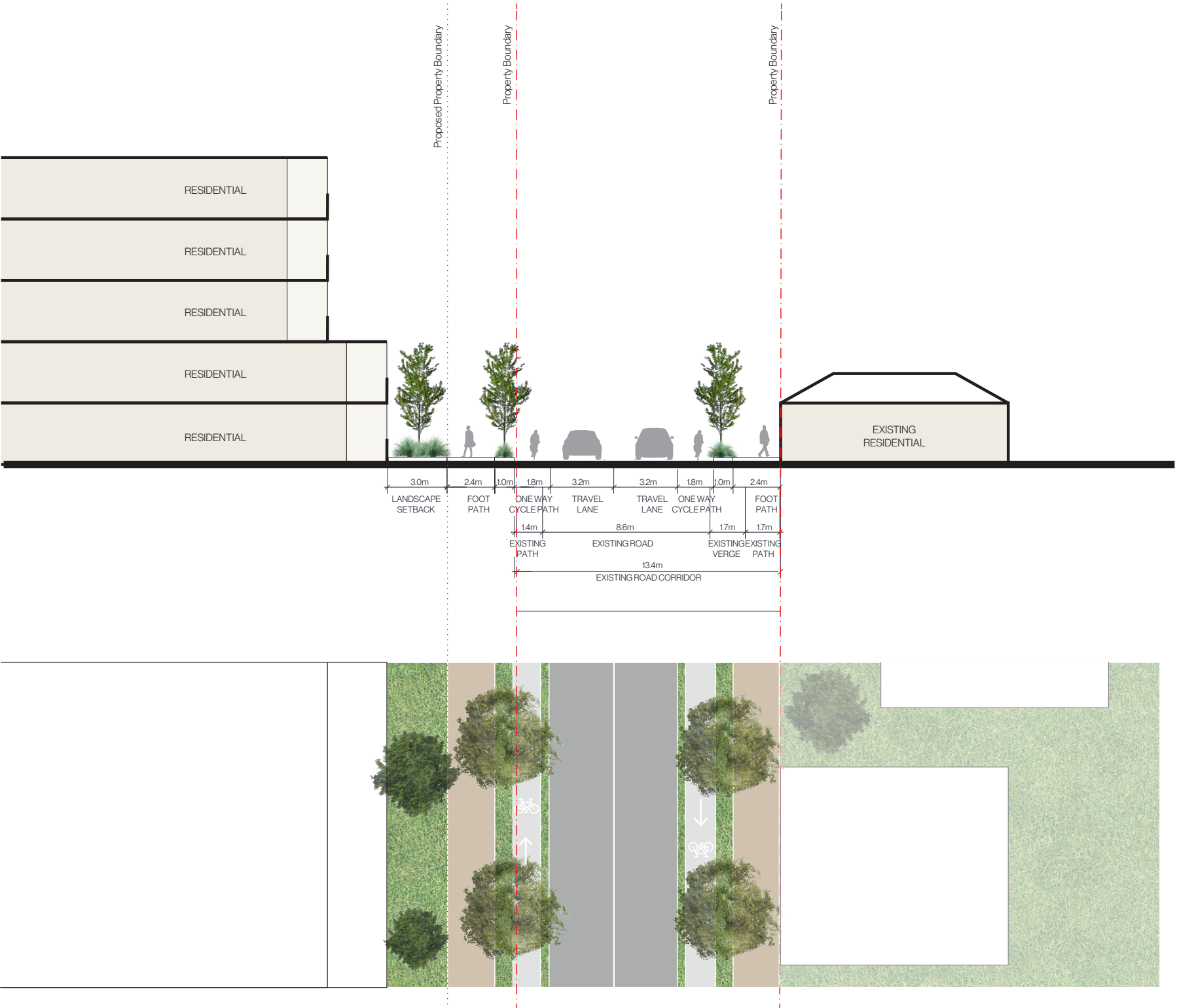
Queens Road forms a main connection towards Five Dock Leisure Centre, Five Dock Indoor Cricket and the Golf Club for the Kings Bay Precinct. A shared path, pedestrian footpath and landscape setbacks and verges enhances this connection to the recreational area, making active transport options accessible and inclusive.

A 6.4m setback from the property boundary will allow for a wider shared path and abundant greenery improving the overall pedestrian experience. The 3m landscape setback within this setback will create a better interface to the proposed shared path and pedestrian pathway.

The 2.4m shared path separated by planting on the southern side of the street will improve how connected the precinct is and improve visibility between pedestrians, cyclists and motorists.

Proposed built form will ensure a smooth transition with the 1 to 2-storey context. This design approach will harmonise the new developments with the existing architectural scale, maintaining the character of the area.

Landscape design along Queens Road will prioritise the integration of trees and green verges increasing the canopy cover along the street. This will enhance the visual appeal and contribute to a more sustainable and comfortable environment for pedestrians.



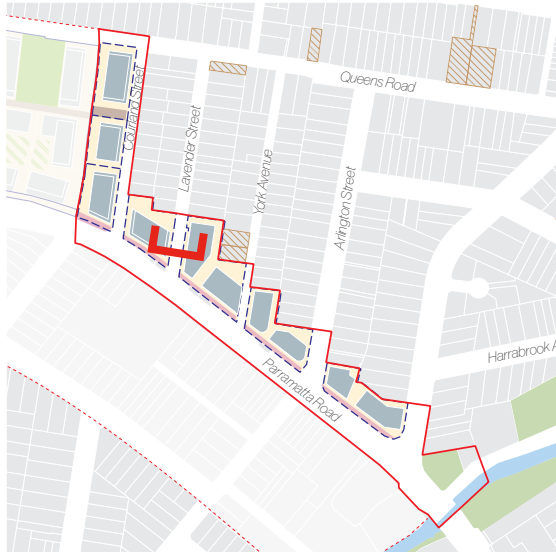
Lavender Street

Lavender Street will feature a row of trees within the 3m landscape setback, extending the percieved width of the street to create a more comfortable path of travel for pedestrian movement.

Part of the existing footpath that is currently within the property boundary on the eastern side of Lavender Street will be dedicated to Council to retain the existing width of the footpath. A proposed 3m setback from the property boundary on both sides of the road will permit the development of abundant greenery.

A 4-storey street wall has been designed to sympathetically interface with the scale of the street. The transition of height from Parramatta Road responds to the single storey, detached residential properties to the north.

Buildings along Lavender Street will accommodate predominantly residential uses, with ground floor retail space proposed along Parramatta Road.



York Avenue

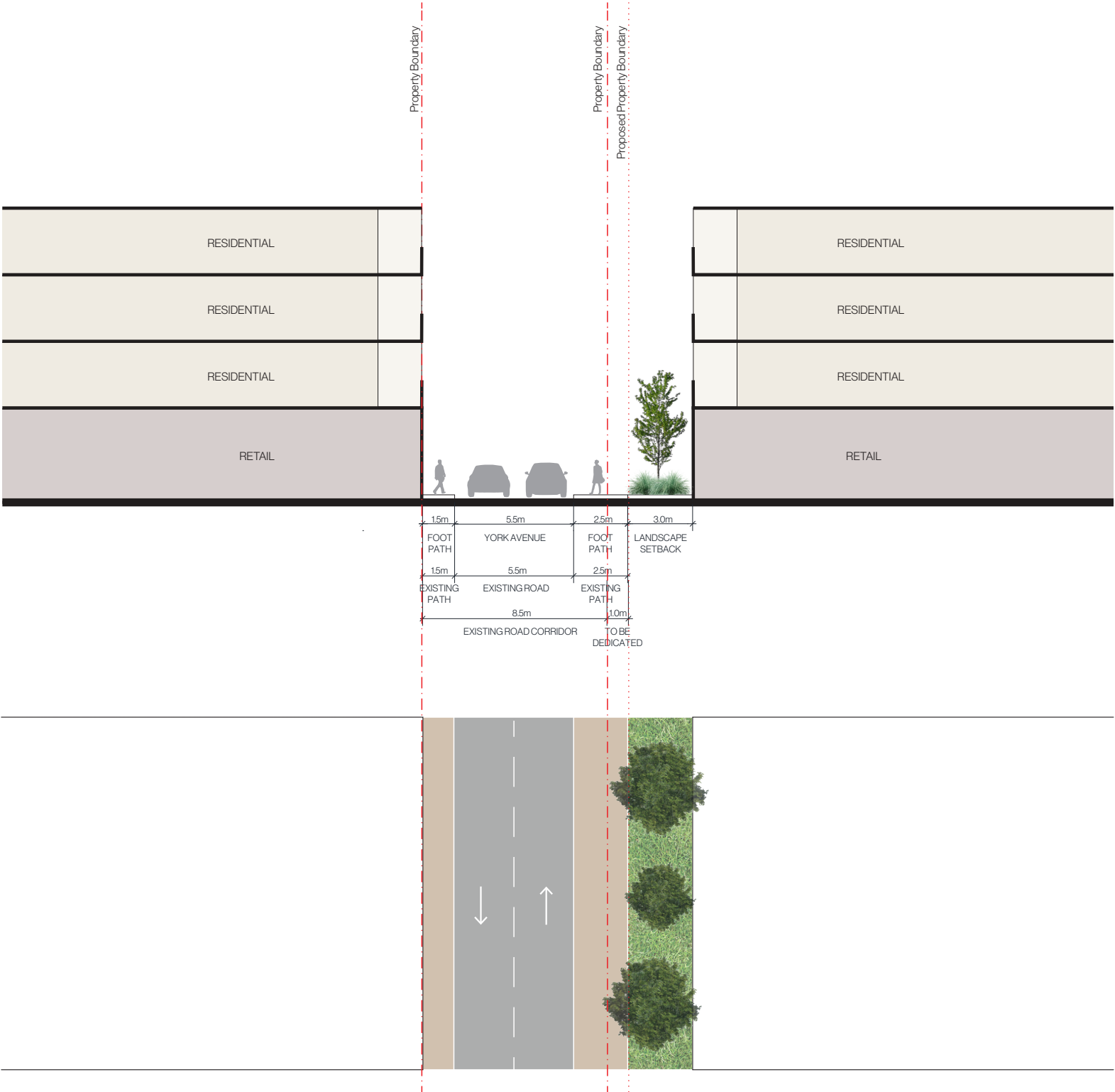
York Avenue is characterised by a two lane road corridor, a row of street trees on the east and a 0m setback to the west, matching the setback of the adjacent heritage character of the street.

Part of existing footpath within existing property boundary on the eastern side of York Avenue will be dedicated to Council to retain the existing width of the footpath.

A proposed 3m setback from the new property boundary on the eastern side of the road will permit the development of abundant greenery. Proposed built form on the western side of the road has a 0m setback to align with the heritage frontage at 3 York Avenue to its immediate north.

A 4-storey street wall has been designed to sympathetically interface with the scale of the street. The transition of height from Parramatta Road responds to the single storey, detached residential properties to the north.

Buildings along York Avenue will accommodate predominantly residential uses, with ground floor retail space proposed along Parramatta Road.



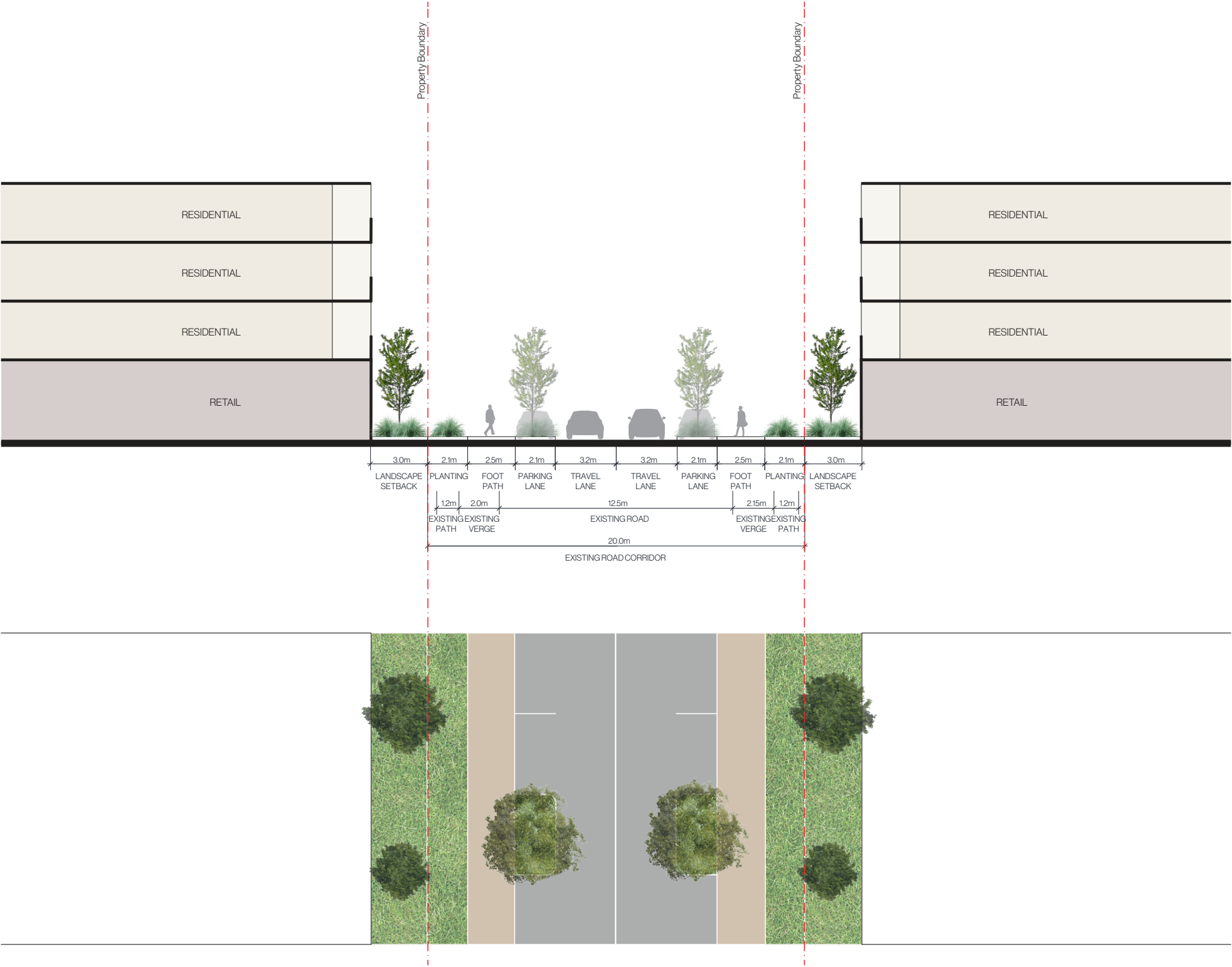
Arlington Street

Arlington Street has a boulevard character with two rows of trees within variable planter/ parking beds and landscape verges lining a two lane road corridor.

A proposed 3m setback from the existing property boundary on both sides of the road will provide more space for landscape connections with mature tree canopy.

A 4-storey street wall has been designed to sympathetically interface with the scale of the street, providing transition of height from Parramatta Road to the single storey, detached residential properties to the north.

Buildings along Arlington Street will accommodate predominantly residential uses, with ground floor retail space proposed along Parramatta Road.



Great North Road

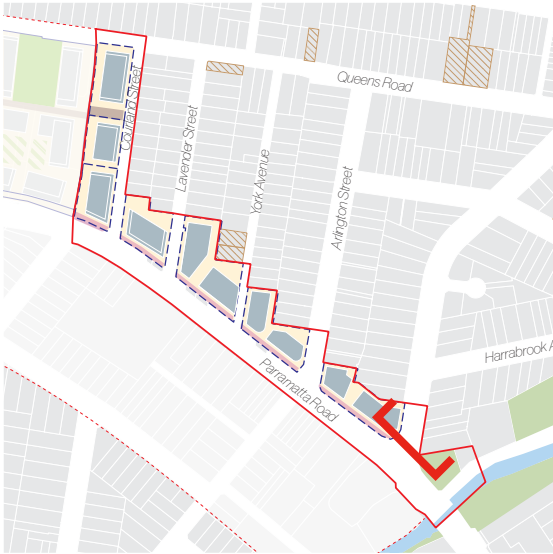
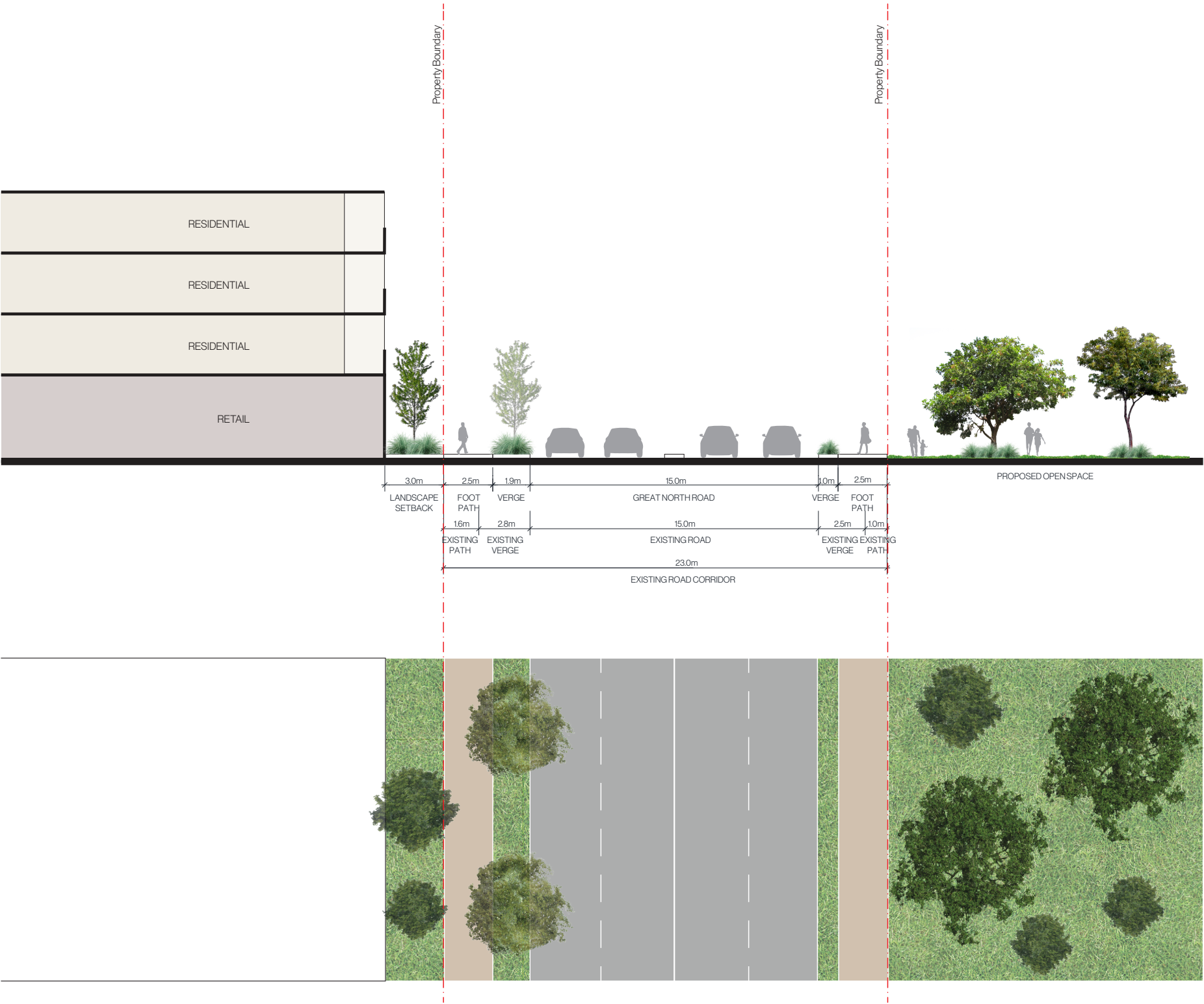
Great North Road is characterised by landscape verges buffering pedestrian movement from the four lane road corridor.

A proposed 3m setback from the property boundary will provide a landscape belt with increased tree canopy contributing to an improved pedestrian experience and enhanced green links.

A 4-storey street wall has been designed along the west to sympathetically interface with the scale of the street.

A new local park will connect to the open-space network that stretches along Iron Cove Creek, creating a direct link to Timbrell Park and Parramatta River beyond.

Buildings along Great North Road will accommodate predominantly residential uses, with ground floor retail space proposed along Parramatta Road.



Parramatta Road East

The character of Parramatta Road has been designed to integrate the street’s existing role as a major thoroughfare with the proposed retail and commercial hub of Kings Bay.

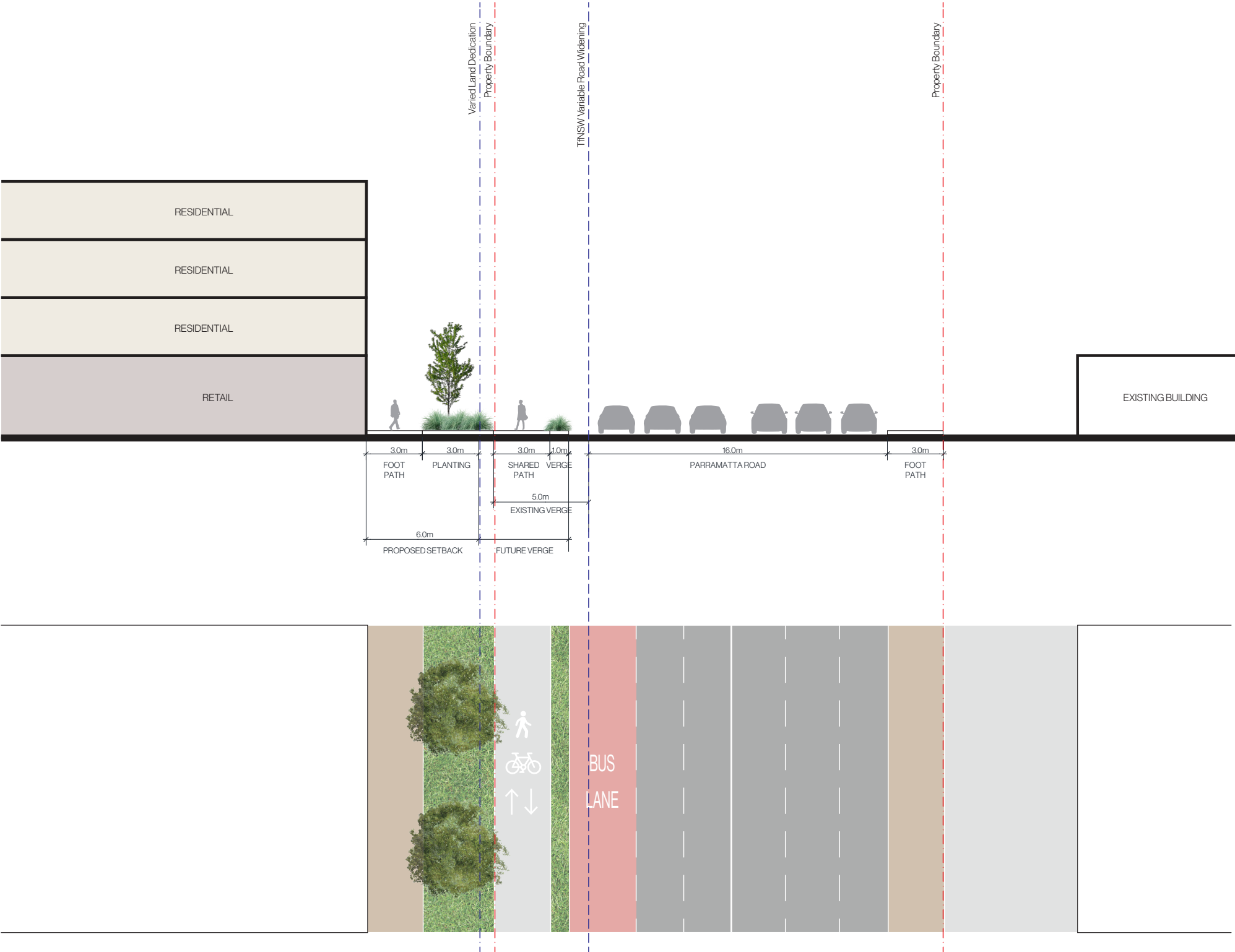
A proposed shared path and pedestrian footpaths will enhance transport infrastructure along the street. Significant portions of landscaped planting areas will be accommodated within a wide setback of 6m and the variable road widening (TfNSW).

This setback will not only provide a sense of relief for residents and pedestrians but also facilitate a smooth transition from the bustling environment of Parramatta Road.

The development along Parramatta Road east will feature a consistent 4-5 storey street wall in the Kings Bay Precinct transitioning in height towards the new local park. These buildings will incorporate ground floor retail spaces and commercial offices, effectively supporting the business activities of the corridor.

In areas prone to flooding, it is mandated that buildings have their ground floors elevated to 400mm above the existing ground level.

*Note: Refer to page 40 for Variable TfNSW Road Widening



Courland Street Link

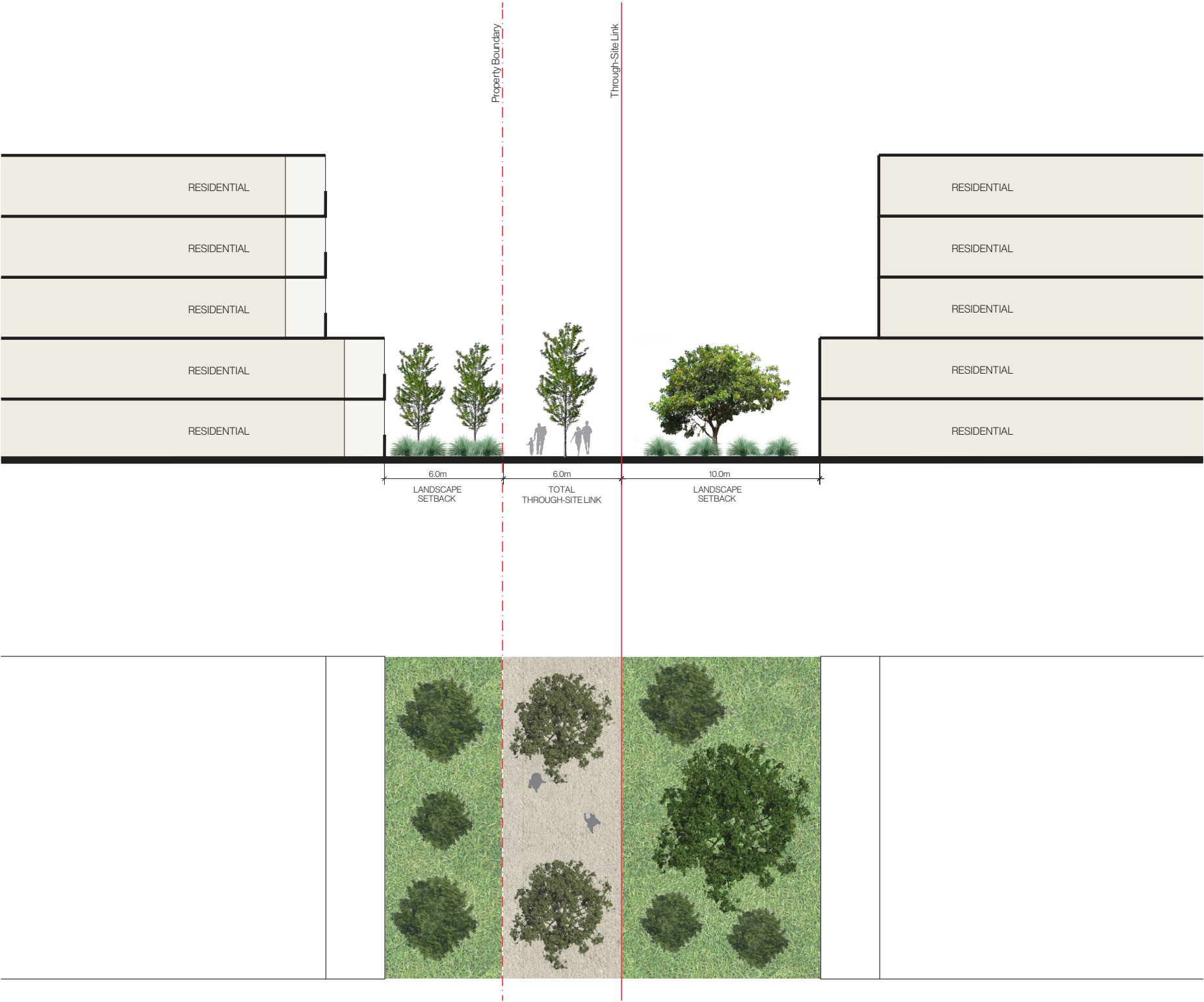
Courland Street Link will be a pedestrian link characterised by a mature tree canopy and dense planting shading pedestrian movement and contributing to mitigation of heat island effect.

The link will feature a flush, paved ground surface to delineate the link as a pedestrian zone.

A proposed 6m and 10m landscape setback from the property boundary on both sides of the through site link will provide space for ecological connections, enhance passive surveillance and improve connectivity by generating a comfortable pedestrian environment through the development.

A 2-storey street wall along the through site link has been designed to sympathetically interface with the scale of the pedestrian street and provide a human-scale streetscape.

Buildings along the through-site link will accommodate residential uses.







7.0 YIELD SCHEDULE

07

YIELD

PARRAMATTA ROAD KINGS BAY WEST PRECINCT

GroupGSA for City of Canada Bay Council

Description	Date	Revision
Revised Masterplan	2/9/2022	A
Revised Masterplan	19/4/2023	B
Revised Masterplan	20/2/2024	C
Revised Masterplan	5/6/2024	D

Table 1: Study Area Summary OPTION 1		
Overall Site Area:		17,009 sqm
TOTAL GFA		32,757 sqm
TOTAL Dwellings		365 units

Development Assumptions		
Building Efficiency (Non-Resi)		
GFA / GEA Ratio: Retail / Commercial		75%
Apartments		
GFA / GEA Ratio		75%
NSA / GFA Ratio		85%
GFA / GEA Ratio (Alt Calc, GFA excludes shared circulation)		75%
Apartments: Typical Unit Size		
	1B	55 sqm (NSA)
	2B	77 sqm (NSA)
	3B	105 sqm (NSA)
Average (Assumption used for Alt Calc)		85 sqm (GFA)
Assumed NSA Mix		20% 1B 60% 2B 20% 3B 100%

PARRAMATTA ROAD KINGS BAY EAST PRECINCT

GroupGSA for City of Canada Bay Council

Description	Date	Revision
Revised Masterplan	2/9/2022	A
Revised Masterplan	19/4/2023	B
Revised Masterplan	20/2/2024	C
Revised Masterplan	5/6/2024	D

Table 1: Study Area Summary OPTION 1	
Overall Site Area:	18,511 sqm
TOTAL GFA	28,883 sqm
TOTAL Dwellings	306 units

Development Assumptions		
Building Efficiency (Non-Resi)		
GFA / GEA Ratio: Retail / Commercial		75%
Apartments		
GFA / GEA Ratio		75%
NSA / GFA Ratio		85%
GFA / GEA Ratio (Alt Calc, GFA excludes shared circulation)		75%
Apartments: Typical Unit Size		
	1B	55 sqm (NSA)
	2B	77 sqm (NSA)
	3B	105 sqm (NSA)
Average (Assumption used for Alt Calc)		85 sqm (GFA)
Assumed NSA Mix		20% 1B 60% 2B 20% 3B 100%

Kings Bay West Precinct

DEVELOPABLE LAND (on the non-developed parts of the Study Area)															
Lot	Land Use	Lot Area (m2)	PRCUTS FSR REF	New FSR	Footprint (m2)	Building Storeys	GEA (m2)	Land Use (GFA, m2)		TOTAL GFA (m2)	Units	TOTAL RESIDENTIAL NSA (m²)	Unit Mix		
								Residential	Commercial				1-Bed	2-Bed	3-Bed
LOT A1	Residential	2,396	1.4 :1	1.9 :1	1,213	4 to 6	3,740	4,625	-	4,625	52	3,931	14	31	7
LOT A2	Residential+Commercial	3,246	1.4 :1	1.8 :1	1,426	4 to 6	7,822	5,685	182	5,867	65	4,832	18	38	9
LOT B1	Residential	5,820	1.4 :1	1.9 :1	2,418	4 to 6	14,643	10,983	-	10,983	125	9,335	34	73	18
LOT B2	Residential+Commercial	5,547	1.4 :1	2.0 :1	2,839	4 to 6	15,044	10,825	458	11,283	123	9,201	33	72	18
LOT A TOTAL		17,009		1.9 :1	7,896		41,249	32,117	640	32,757	365	27,299	99	214	52
TOTAL		17,009						32,117	640	32,757	365	27,299	99	214	52

Kings Bay East Precinct

DEVELOPABLE LAND (on the non-developed parts of the Study Area)																
Lot	Land Use	Lot Area (m2)	PRCUTS FSR REF	New FSR	Site Coverage (%)	Footprint (m2)	Building Storeys	GEA (m2)	Land Use (GFA, m2)		TOTAL GFA (m2)	Units	TOTAL RESIDENTIAL NSA (m ²)	Unit Mix		
									Residential	Commercial				1-Bed	2-Bed	3-Bed
LOT C1	Residential	2,576	1.4 :1	1.5 :1	46%	1,192	2 to 5	5,135	3,851	-	3,851	44	3,274	12	26	6
LOT C2	Residential	1,703	1.4 :1	1.6 :1	49%	839	2 to 5	3,688	2,766	-	2,766	31	2,351	9	18	4
LOT C3	Residential+Commercial	2,048	1.4 :1	1.8 :1	51%	1,054	4 to 5	5,000	3,569	181	3,750	41	3,034	11	24	6
LOT D1	Residential+Commercial	2,391	1.4 :1	1.6 :1	45%	1,070	4 to 5	5,080	3,512	299	3,810	40	2,985	11	23	6
LOT E1	Residential+Commercial	4,013	1.4 :1	1.5 :1	51%	2,052	4	8,208	5,735	422	6,156	65	4,874	18	38	9
LOT F1	Residential+Commercial	2,839	1.4 :1	1.4 :1	48%	1,351	4	5,404	3,622	431	4,053	41	3,078	11	24	6
LOT G1	Residential+Commercial	2,941	1.4 :1	1.5 :1	51%	1,499	4	5,996	3,923	575	4,497	44	3,334	12	26	6
TOTAL		18,511		1.6 :1	49%	9,057		38,510	26,976	1,907	28,883	306	22,930	84	179	43
TOTAL		18,511	- :1	1.6 :1	49%	9,057		38,510.4	26,976	1,907	28,883	306 units	22,930	84	179	43

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